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CALLAWAY CENTRAL CORRIDORS AN URBAN DESIGN PLAN Volume #1 - The Urban Design Plan

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PROJECT INTRODUCTION

Overview

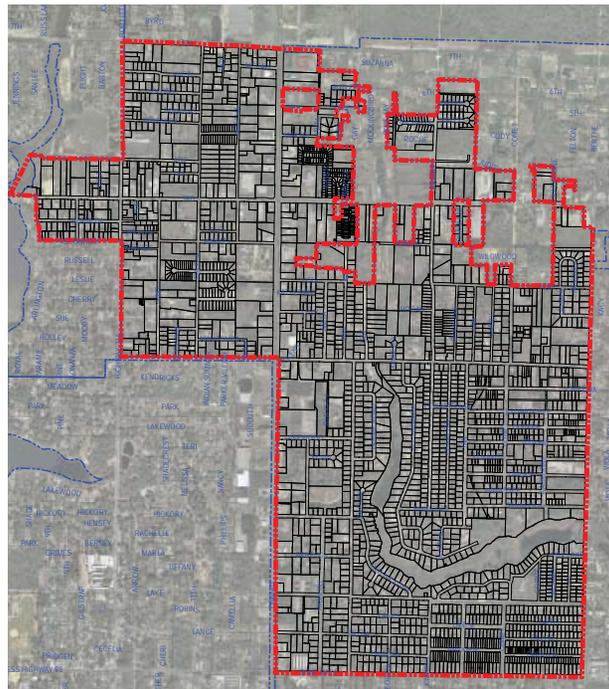
Community Redevelopment Agency (CRA) Overview

The State of Florida provides a means for communities to revitalize the run-down areas in their cities by creating an organization called a Community Redevelopment Agency and a funding source called Tax Increment Financing. The Callaway Community Redevelopment Agency (CRA) was founded in May, 2007. The CRA boundaries were determined by evaluating the existing conditions of the community and establishing an area of slum and blight, as defined in the Florida State Statutes, Section 163.340. The primary goal of the Callaway CRA is to identify strategies and develop plans for energizing the economic vitality of the City and to increase the quality of life for the residents within.

The CRA area is located in the northwest portion of the city. The boundaries are: 7th Street on the north, Boat Race Road on the south, Arlington Street on the west, and Berthe Avenue on the east. The CRA consists of approximately 1,402 acres of which approximately 1,281 are developed land and the remaining 121 is comprised of public rights of way such as: streets and drainage easements. The focus of the CRA area is the commercial corridors of the City: Tyndall Parkway and Highway 22. These two corridors have become rundown and blighted over the years. Many of the buildings are vacant and have fallen into disrepair. The poor stormwater infrastructures, and lack of sidewalks and parking lots have contributed to the blight of the two corridors. The CRA was formed to provide a means to redevelop these important urban corridors in the city in order to increase economic activity in the city and to generate increased tax revenues to provide critical City services.

The funding mechanism for the CRA is called

tax-increment financing (TIF). These revenues come from increased property values as a result of the redevelopment efforts in the CRA area. The CRA may utilize bonds to build critical capital improvements for the City in order to “spur” private investment in the CRA area. The types of capital improvement projects and their locations were determined during the development of the CRA Plan. This plan has become a road map for orchestrating the revitalization of the CRA area. TIF monies can only be spent on project types outlined in the redevelopment plan.



Community Redevelopment Agency (CRA) Boundaries

Purpose of this Project

The IBI Group was hired by the City of Callaway to establish an urban design framework for the future redevelopment of the central corridors in the City. This plan focuses on one major goal: 1. Defining a community compatible character for public spaces and private development architecture. The community was involved in a series of engaging planning and design workshops with the express purpose of providing direction for the creation of a common vision for the future of the design of the their revitalized commercial center. The **objectives for the plan** are as follows:

1. To create a future urban design vision for the commercial corridors of the City.
2. To create a “Town Center” for the City.
3. To ensure quality future redevelopment land use patterns and architectural quality by providing clearly understandable urban design criteria.
4. To develop recommendations for revisions to the Land Development Code to effect the urban design plan.
5. To provide a funding strategy to realize the redevelopment of the plan.



Community perceptions

Study Area

The study area for the project was established as approximately one block either side of Tyndall Parkway and Highway 22 within the CRA boundaries. The study area is: One block either side of Tyndall Parkway from 7th Street to Boat Race Road within CRA area. Another study area is the one block around along Highway 22 from the west city limits, Arlington Drive, to Berthe Avenue on east within CRA area.



Tyndall Parkway looking into an intersection of Cherry Street



Highway 22 looking into the west

Study Background and Process

The planning process timeframe for this study was 7 months which included a series of 4 milestone public involvement workshops. The process began in October of 2008 and ended in May of 2009. The following is the process that was used for the project:

Work Task 1.0 – Project Organization

1. Establish a detailed work plan and schedule for the project.
2. Develop a public involvement process and schedule.

Work Task 2.0 – Base Mapping and Data Inventory

1. Review of all pertinent planning studies that are available for the corridor area.
2. Visit the site to collect existing site conditions data.
3. Prepare GIS maps and inventory data maps for the (2) corridors

Work Task 3.0 – Site Analysis

1. Analyze existing site conditions, character and function of the major corridors.
2. Identify issues, concerns, opportunities, and constraints for the project.

Work Task 4.0 – Stakeholder Input and Community Visioning

1. Staff meetings and public meetings were held to solicit in-put of concerns and issues from the property/business owners and the citizens of Callaway.
2. Public meetings were held in November 2008 to obtain future design visions from the community.
3. A workshop was held to present the plan to the public in order to receive comment on the

- conceptual plan elements.
4. A final workshop was held to receive input on the final plan.

Work Task 5.0 – Urban Design Concept Plan

1. Creation of a conceptual urban design plan to establish the framework for development and redevelopment along the corridors.
2. The conceptual plan addresses both public sector design criteria for the roadway ROW and park lands as well private development opportunities adjacent to the public ROW.

Work Task 6.0 – Final Urban Design Master Plan

1. Preparation of the final urban design plan including graphics, maps and implementation strategies addressing the generalized requirements for future redevelopment activity in the corridors.
2. Capital Improvements Plan
3. Cost Estimates
4. Phasing Plan
5. Funding Plan
6. Code Revisions
7. Urban Design Guidelines

Work Task 7.0 – Final Documentation and Plan Adoption

1. The final plan was reviewed and approved by the CRA Board in May 2009



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THE CORRIDORS URBAN DESIGN CONCEPT

Overview

The urban design concept plan has evolved out of a series of strategies that were originally put forth in the Community Redevelopment Plan and then refined in the public involvement process for this urban design plan. The goal for the urban design concept is to provide a planning guideline for the City of Callaway planning staff to put in place a series of planning and design actions that will help promote the goals and objectives for redeveloping these corridors as outlined in the CRA Plan. The CRA Plan dealt with the urban planning strategies necessary to revitalize these corridors. This plan focuses on creating a design image and character for the public and private spaces that are to be created as a result of the redevelopment effort. In addition, this plan provides a means for promoting, guiding, and regulating these urban design principles as the redevelopment process unfolds. The following set of strategies formed the basis for the development of the urban design concept.

Economic Strategy

The successful future redevelopment of the Callaway corridors relies on the implementation of both internal actions and external actions. The external strategies entail putting in motion actions to promote regional economic development. The internal strategies entail providing incentives to attract private development investment into the region as well as to increase demand for local goods and services in the corridors.

1. External Strategies

- The City and CRA should work with the Bay County Chamber of Commerce to promote the Airport Business Park to create job opportunities that attract new residents and businesses to Callaway.
- The City and CRA should work with the Bay County Chamber of Commerce to attract industrial and manufacturing companies to the regional area to create job opportunities that attract new residents and businesses to Callaway.
- The City and CRA should work with the Bay County Chamber of Commerce to attract more tourism and retirement visitors to the region.
- The City and CRA should work with the Air-Force Base to promote increased base activity and growth.

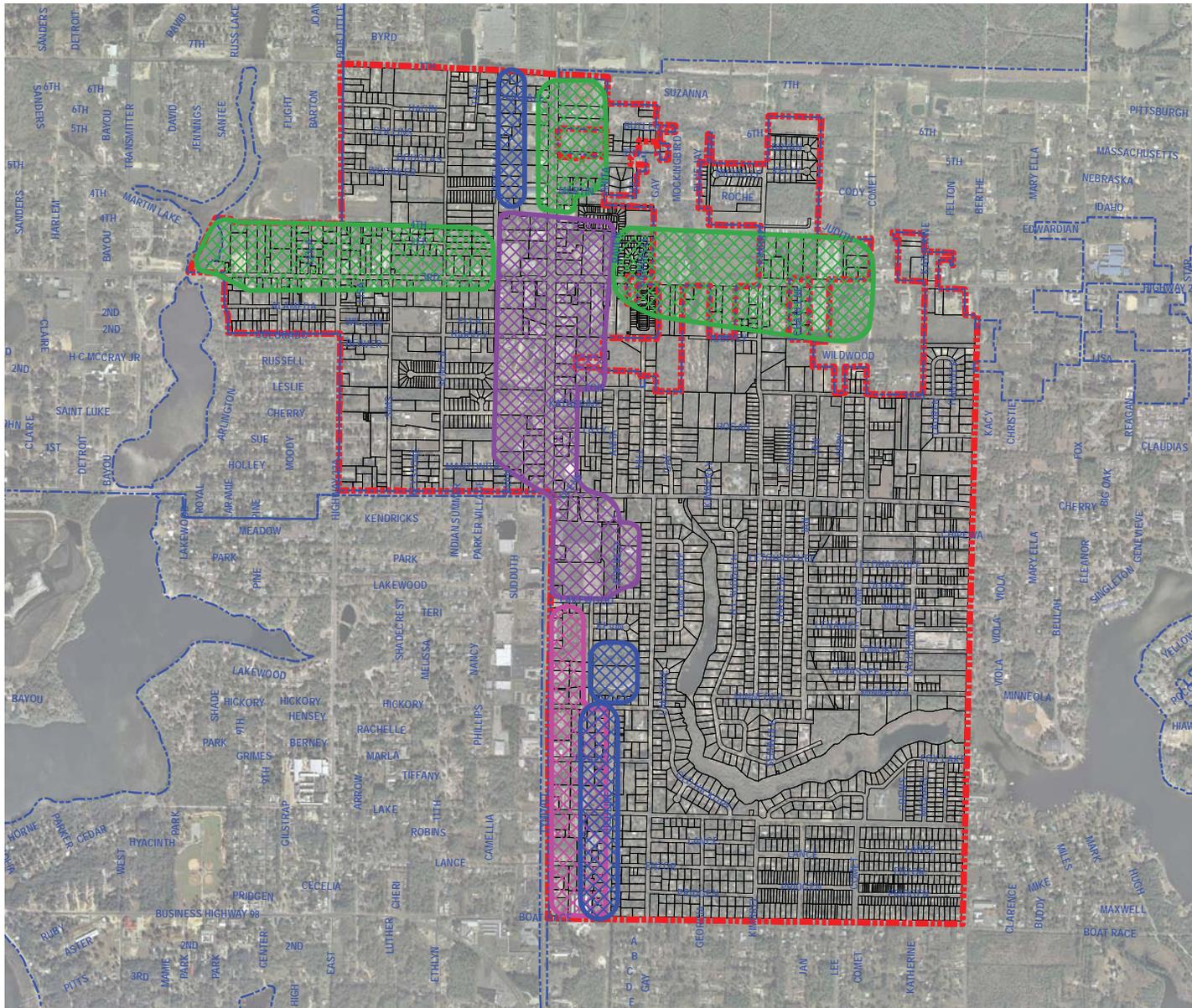
2. Internal Strategies

- Provide appropriately zoned land to attract developers through opportunity.
- Provide up-dated and modernized public infrastructure to attract developers.
- Create a beautiful commercial town center via landscape improvements to attract residents and businesses.
- Create a quality of life in the community that attracts residents to the region.
- Create quality transit facilities and routing for increased attractiveness of the corridors.

- Provide quality mixed-use residential opportunities to create consumer demand in the corridors.

The economic strategy for the corridors is to create specialized mixed-use land use nodes in the 2 corridor area to attract similar businesses and to encourage a business synergy amongst them.

Callaway CRA Conceptual Plan



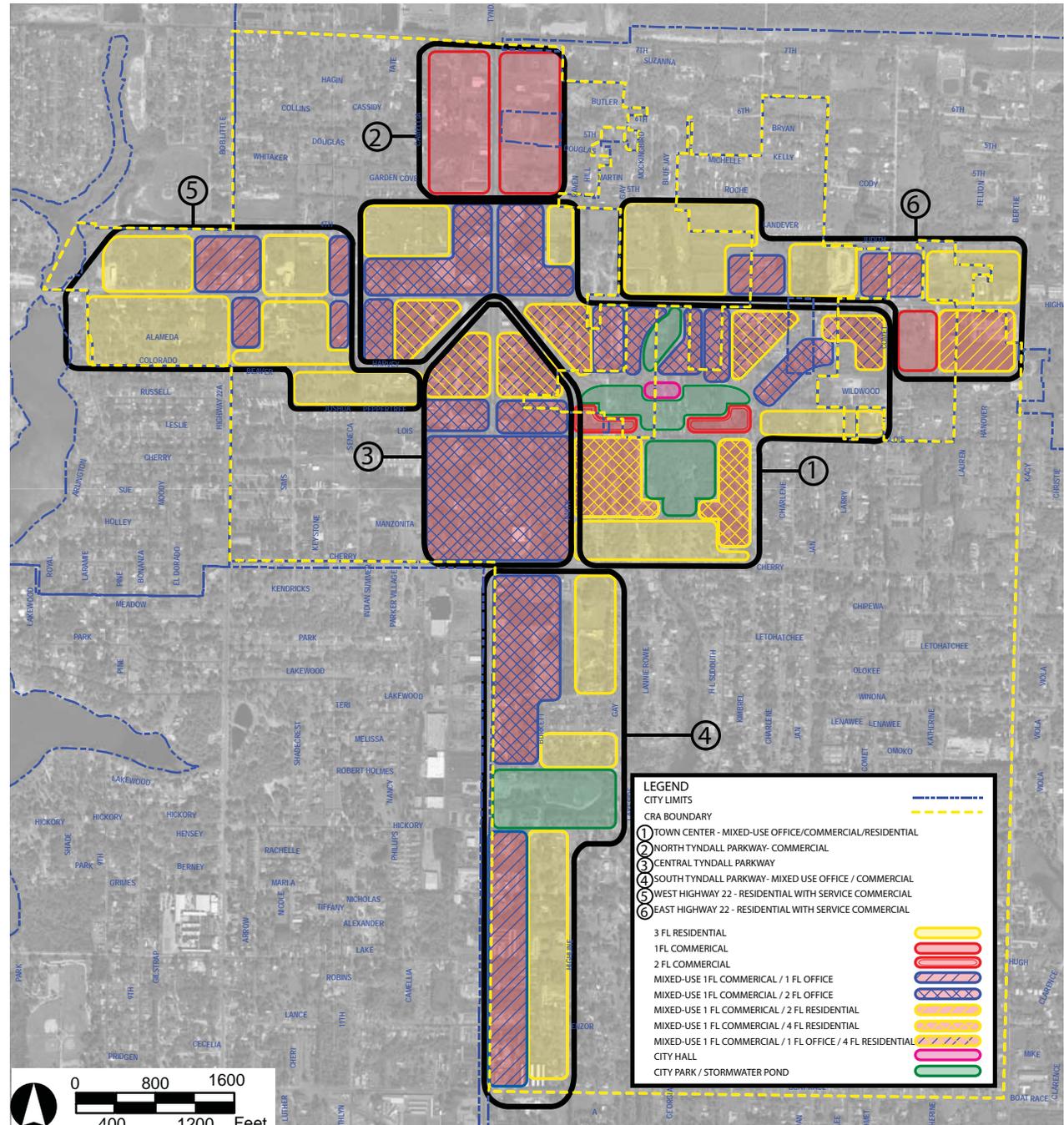
- City Limits
- CRA Boundary
- Multi-Family Residential
- Mixed-Use
- Commercial with Multi-Family Residential Potential
- Mixed-Use Office



Land Use Strategy

The land use strategy for the corridor concept plan implements the economic revitalization recommendations listed in previous pages. The land use concept plan is based upon the recommendations of the CRA master plan, which incorporates the complete redevelopment of the 2 central corridors of Callaway and the creation of a new “Town Center”. This is a long range vision plan which envisions the total transformation of the corridor over many decades. On Highway 22, the plan envisions the roadway widened to a 4-lane section and the rezoning of the adjacent land to encourage the development of new multi-family residential communities and new residential/commercial mixed-use projects. On Tyndall Parkway, the plan envisions the reconfiguration of the 4-lane section roadway to incorporate access management improvements and the redevelopment of the adjacent parcels into office/commercial mixed-use development. The current zoning does not allow for any mixed-use development in the corridor. The center piece of the redevelopment plan is the creation of the Town Center. The town center will be located in the southeastern quadrant of Highway 22 and Tyndall Parkway. This project will be the new commercial and civic center for the City. As a result of these proposed changes, there will need to be a re-writing of the zoning code and a rezoning of the corridor to allow for this vision. The success of the redevelopment effort requires increases in densities along the corridor in order to create the critical mass required to generate the purchasing power to: support a new mixture of retail tenants in the corridors, enable multi-modal transportation, and to attract developers to the area. There is not the market to demand this type of development at the current time. It will be necessary for the CRA to make the necessary public upgrades in beautification, parks and storm water management in order to attract new residents, retailers, and developers to the corridors.

Corridor Urban Design / Land Use Concept Diagram



The following is a description of the 6 major land use districts in the corridors and their future land use designations.

District #1 – Town Center

- Encourage the mix of all land uses; residential, commercial, office, institutional, open space, and public purpose. Create an attractive and vibrant public square and commons for events, performances, festivals, and civic activities. The focal point of the town center will be the series of “central park” ponds that will be designed as storm water treatment facilities and a flood mitigation program. Some of the types of commercial businesses that are desired in this zone are: boutiques, clothing, jewelry, books, restaurants, outdoor café, and so on. Drive-thrus, gas stations, automobile repairs, would not be allowed in this zone in order to maintain aesthetic quality and pedestrian safety.

District #2 – North Tyndall Parkway

- This district should be designed to encourage continued in-fill redevelopment with “big-box” / major anchor retailers. In addition, the district should provide an opportunity for transitional multi-family housing between the commercial and the single family neighborhoods behind. The district covers the areas along North Tyndall Parkway between 5th Street and 7th Street. One and two floors of commercial land use in this zone functions as transition into existing “big-box” retails and “Galleria” project outside of CRA boundaries. The types of businesses that should be encouraged in this area would be: grocery stores, electronics, books, pets, drug stores, restaurants, home furnishings, product show cases, etc.

District #3 – Central Tyndall Parkway

- Central Tyndall Parkway will have mixed-use retail, office, and residential above with a “Main Street” orientation towards Tyndall Parkway.

This section of the corridor will have a major transit station and act as a multi-modal hub for the town center.

District #4 – South Tyndall Parkway

- The south end of the Tyndall Parkway corridor should be designed to encourage more office and office-related uses. The desire is to promote mixed-use development in this district which has commercial business on the bottom floor and offices above. This district should also have transitional multi-family units between the mixed-use office frontage and the single family neighborhoods behind. This district is located between Cherry Street and Boat Race Road on Tyndall Parkway and extends a block to the east and west of the parkway. The western side of the parkway in this district is in the City of Parker. For this reason, it is highly recommended that City of Callaway and City of Parker coordinate the development of compatible urban design guidelines along South Tyndall Parkway. The buildings in this district would be mixed-use: first floor commercial with 2 – 3 stories of office above. The multi-family residential component would be a 3 story product. The ideal commercial land use types would be: office supplies, pharmacy, cleanings, café/ restaurants, etc. The ideal office uses would be financial, insurance, professionals, real estate, etc. This district would also contain a City park/ storm water pond facility to treat existing storm water run off on its way to the Bayou.

District #5 - West Highway 22

- The western section of Highway 22 will focus on multi-family residential development with corner nodes of service commercial uses. The district will encourage 3-story, multi-family residential communities on both sides of Highway 22. Development in these districts should also be oriented toward the existing neighborhoods by providing connectivity to

neighborhood commercial services. Service commercial businesses should include the following: day-care, dry-cleaning, convenience stores, and so on. Residential development types should be multi-family residential products such as town homes, condominiums, and apartments.

District #6 – East Highway 22

- The eastern section of Highway 22 should focus on multi-family residential development with corner nodes of service commercial uses. The district will encourage 3-story, multi-family residential communities on both sides of Highway 22. Development in these districts should also be oriented toward the existing neighborhoods by providing connectivity to neighborhood commercial services. Service commercial businesses should include the following: day-care, dry-cleaning, convenience stores, and so on. Residential development types should be multi-family residential products such as town homes, condominiums, and apartments.

Transportation Strategy

The corridor goal for the redevelopment of the roadway systems is to establish an efficient circulation system and safe accessibility for vehicles and pedestrians alike. The elements addressed in this section are the following: roadway system, building frontage access, public transportation, and parking.

Roadway System

The renovation of the major roadways and secondary grid is an important element in the urban design concept plan. There are 2 major objectives to be accomplished by the revitalization of the road system. The first objective is to increase safety and roadway capacity. The second objective is to prepare for a managed access system into new and redeveloped properties along the corridor in order to fit the revised zoning code.

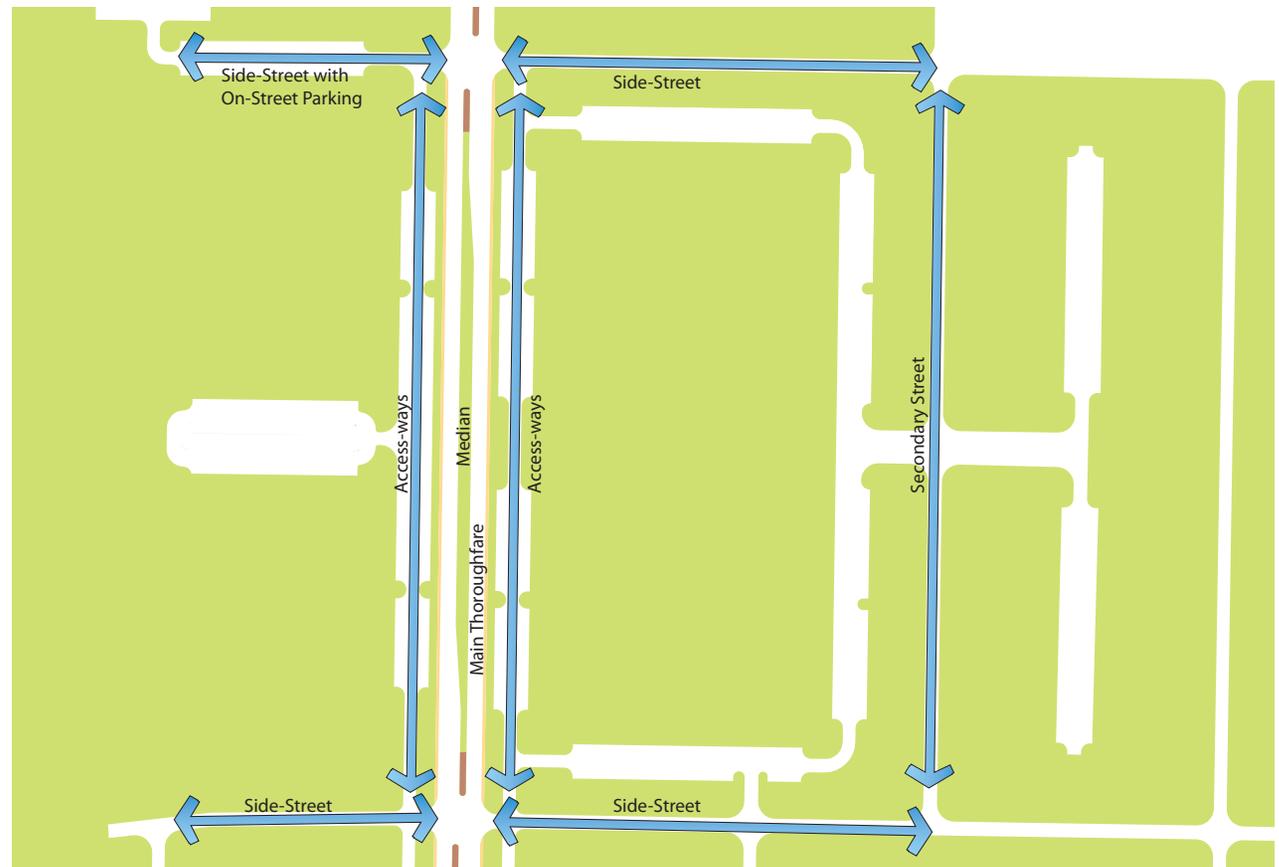
Safety and Roadway Capacity

Currently, there are numerous opportunities for left-hand turns along Tyndall Parkway and unlimited opportunities along Highway 22. These left-hand turns are the cause of numerous accidents from conflicts with on-coming vehicles. The plan proposes to consolidate and move all left-hand turn movements to intersection locations. The medians would be closed on Tyndall Parkway and medians will be added on Highway 22. In addition to left-hand turn management, the plan will also encourage the closure of the business access curb cuts directly onto the main thoroughfares. These access ways are another source of accidents. In addition to being a safety hazard, the constant access traffic significantly reduces the capacity of these major roadways.

Re-development Access Management

The access management plan for redevelopment focuses on the creation of a new business access system and business connectivity plan. If the main curb-cuts are eliminated, there will be a need for a new business access system. Business access will be re-organized to come from the side-streets. Customers will be directed to the intersections where they will make a turn onto a side street. From the side-street, they will turn into adjacent parcels of land either in front of the buildings or behind the buildings (depending on the district). A new connected access-way (frontage-road or alleyway) will be created to allow automobiles to traverse the total length of the block to the next intersection /

side-street, where they may exit the commercial center and safely enter the major thoroughfare via the intersection. These new common access-ways will need to be introduced in a phased fashion over time in order to create this access continuity throughout a very long redevelopment period. The access-ways will take a lot of vehicular trips off-of the main thoroughfares and allow for consumers to drive safely from one shop to the next in the macro-block area.



Typical block showing relationships of side-street, access-ways, and main thoroughfares

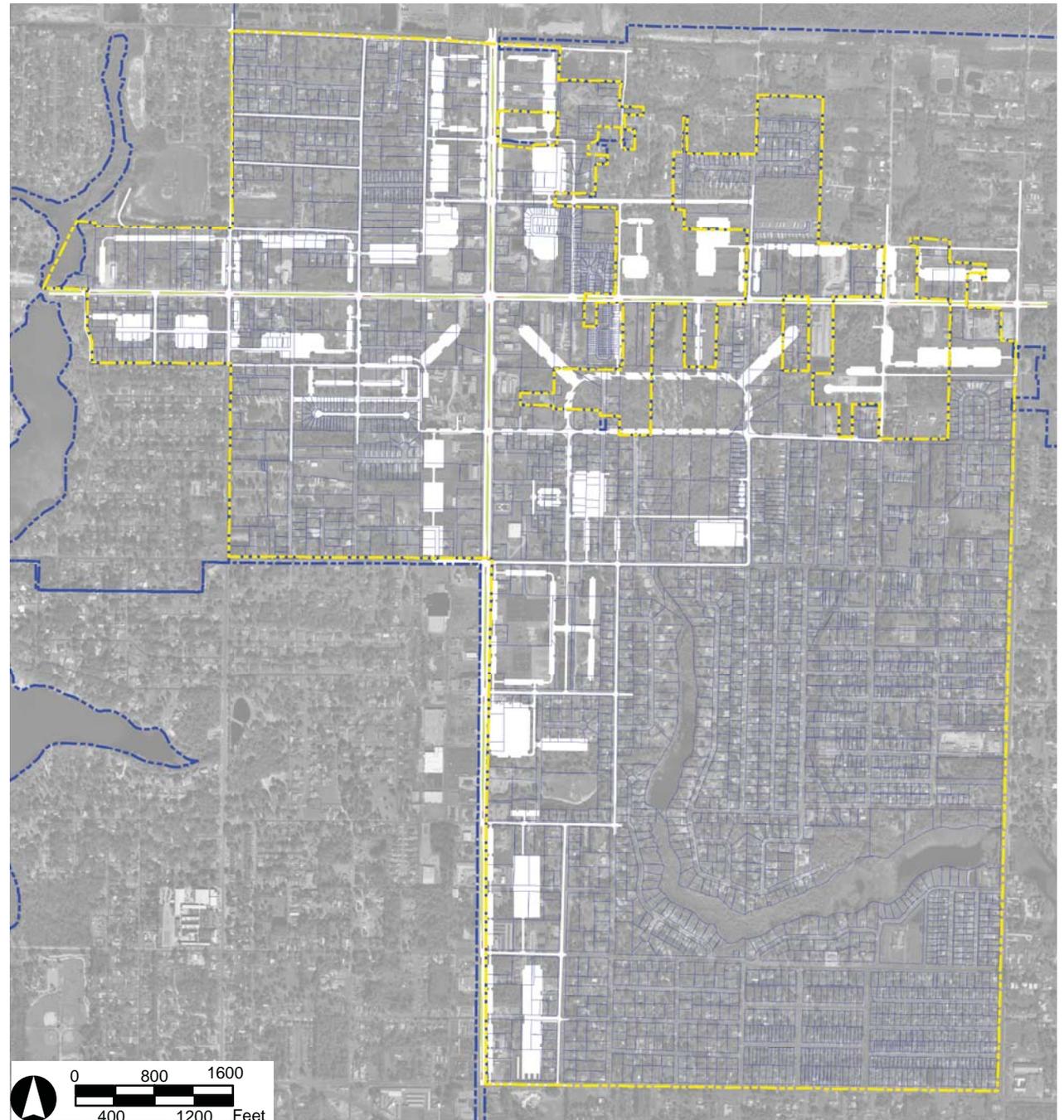
Highway 22

The concept plan recommends expanding the existing right-of-way from 60 feet to 100 feet on the west side of Tyndall Parkway in order to provide for four lanes with a central median. There will be striped bike lanes on both the outside east and west bound lanes, 5 foot sidewalks, and a landscape strip where not adjacent to commercial businesses. Because this is an under-developed corridor at the present time, the access ways can be built as each new property develops along the highway. The side-street access points will need to be built at the same time that the main highway is built.

Tyndall Parkway

The concept plan proposes that the 4-lane divided highway section remain mostly as it is today. The major changes will be to close the medians except at the intersections, and to close the access curb cuts once the access-ways have been built.

Corridor Urban Design / Land Use Concept Diagram



Public Transportation

The public transit system will be a very important element of the urban design scheme. As the higher density developments come on line, the demand for the Bay Town trolley will drastically increase. In order to make the trolley option more popular during the transition time, there needs to be station area improvements that increase the quality of the pedestrian experience related to the trolley stop wait. Some local residential pick-up loops need to be added to the routing in order to collect the resident and take them to the corridors. There needs to be Bay Town trolley that tracks the main corridors and provides easy access to the shops along the access-ways.



Pedestrian oriented typical bus / trolley stops that encourage to use the public transportation

Bikeway System

The concept plan relies on a strong emphasis on bicycles and a well defined bikeway system. There should be striped bikeways on the main thoroughfares as well as the proposed access-ways. The access-way travel speeds will be quite low and allow for in-lane bike travel. All major intersections adjacent to commercial areas will have bike stands and dismount areas. The entrances to major commercial plaza areas should be required to include bike rack facilities. When the town center is developed, there will be a central park designed around the main drainage ways. These drainage ways and central park areas should incorporate bikeways that connect up the on-street facilities.



Typical bicycle rack along bike trails

Parking Locations

The majority of the customer parking for the businesses will be developed as a part of the redevelopment process as called for in the new development code. Predominately the parking will be provided in internal parking lots surrounded by the new buildings. In the higher density districts such as the town center, there will need to be structured parking to accommodate the required cars. Public parking spaces need to be increased also. There will be a big need to have on-street parking in the new town center for increased walkability among the street-level activities. As a result, the plan calls for on-street diagonal parking on all of the future town center streets.



On-street parking and structured parking will be developed in the new town center

Urban Design Strategy

The urban design strategy of the concept plan sets forth the criteria for creating a high quality visual environment for the future development in the central corridors. Currently, one of the biggest deterrents to economic vitality and revitalization is the poor quality of the visual environment in the central corridors. This poor condition will not be changed over night, it will take a long time of public and private investment in the corridors to slowly transform the area. The best way to jump start a revitalization program is to embark on a beautification streetscape improvement program on the main thoroughfares. This kind of beautification program can show the private sector that the public sector cares and that it is safe to invest in the corridor. The other half of the urban design world is the area of private land and businesses adjacent to the roadway. The best way to encourage good quality site and architectural design is to re-write the zoning land development codes to require basic landscape architectural and architectural quality.

Private Sector Guidelines

Site Design

The site design approach for the concept plan is focused on creating a unified mixed-use urban community on the main corridors. In the town center, the site planning philosophy is to create a traditional downtown feeling by organizing a mixture of civic, commercial, entertainment activities on the ground floor levels all oriented toward the central town park. The majority of the parking will be handled with on street parking and mid-block structured parking. The buildings will be sited on the front lot lines with a strong urban streetscape design. On Tyndall Parkway, the site planning for the redeveloped properties will be focused on the reinforcement of the access way concept. The new development will be required to build the frontage road and parking in a fashion that connects to the adjacent parcels. The mixed-use buildings will be

set adjacent to the access way, separated by a wide pedestrian promenade. The promenades will wrap around the buildings with additional pedestrian frontage on the side with on-street parking on these side streets. The majority of the parking and service will be to the rear of the buildings. On Highway 22, the site design concepts will be utilized for the new development that comes into the corridor as a result of the widening of the roadway. The proposed land uses for the corridor are residential and commercial on the corners. The residential developments will have a front yard setback from the roadway for noise buffering. On the corners, the buildings will be set on the front lot lines to create a pedestrian scale neighborhood center atmosphere. There will be parking and access for the commercial facilities along the side streets.

Architectural Massing

The architectural massing for the architecture in the corridors needs to reinforce the mixture of land uses and pedestrian character of each district. The town center area will have the most urban scale and should allow for 6 stories of development. There should be a vertical set backs at appropriate locations as well as horizontal breaks to keep the scale in context. The Tyndall Parkway corridor mixed-use development should be designed to a 3 – 4 story height limitation with vertical breaks at the first floor and numerous horizontal breaks in the building mass along the street block frontage.

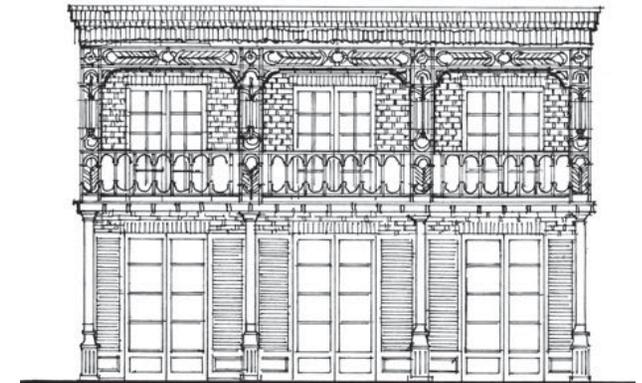


Typical architectural design, Key West

The Highway 22 corridor will have a building height limitation of 3 stories. There should be horizontal breaks in buildings where the land-use composition changes.

Architectural Design

The architectural design styles for the new and in-fill development in the corridors concept plan evolved out of the public workshops and visual preference survey. The preferred design styles are: North Florida Brick Georgian, Brick Victorian Storefront, New Orleans, and Key West. It is important that an equal mix of the styles is incorporated along the corridors. Design guidelines will need to be developed to illustrate how these styles can be incorporated into the height and massing criteria for each district.



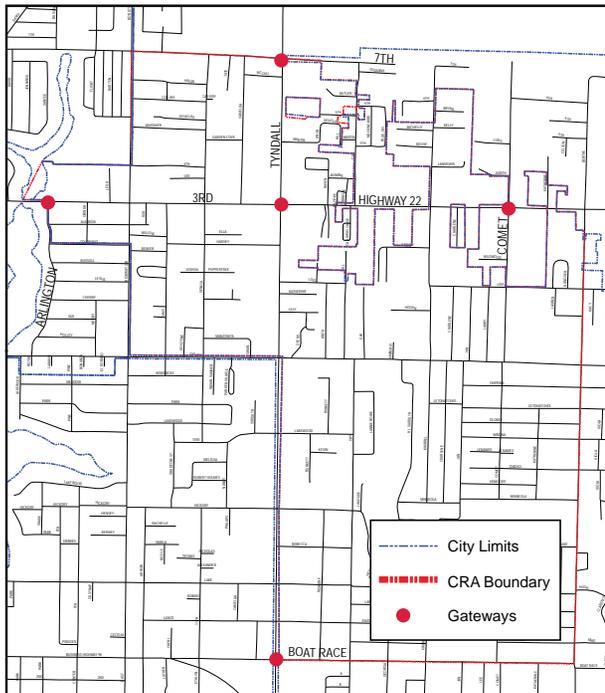
Typical architectural design, New Orleans

Public Sector Guidelines

Gateways

Gateways will be located at important corridor entry points to create a sense of welcome for the commercial district and Town center. The gateways will be located at strategic geographic locations along the corridors. The gateway designs should employ a combination of elements such as landscaping, signage, sculpture, banners, and monuments. Locations for gateways are identified in the plan:

- Intersection of Tyndall Parkway and 7th Street
- Intersection of Tyndall Parkway and Boat Race Road
- Intersection of Highway 22 and Arlington Street
- Intersection of Highway 22 and Comet Avenue



Gateway location map

Pedestrian Plazas and Town Center Commons

Public spaces such as urban plazas, pocket parks, community gardens, town squares, and courtyards are very important components of good urban design and are incorporated in the corridors concept plan. As the density of development increases, the importance for urban oases becomes paramount. The town center will have a central park designed as its center-piece. There will be a series of pedestrian and bikeway connections from the central park to a network of public spaces throughout the corridors. The mixed-use developments will incorporate public plazas, courtyards, and pocket-park spaces. On Tyndall Parkway, there will be a major urban park space designed around the tributary to Callaway Bayou. This park space will have a storm water treatment system incorporated into it.



Public spaces are important elements for high density urban area

Streetscape and Public Landscape

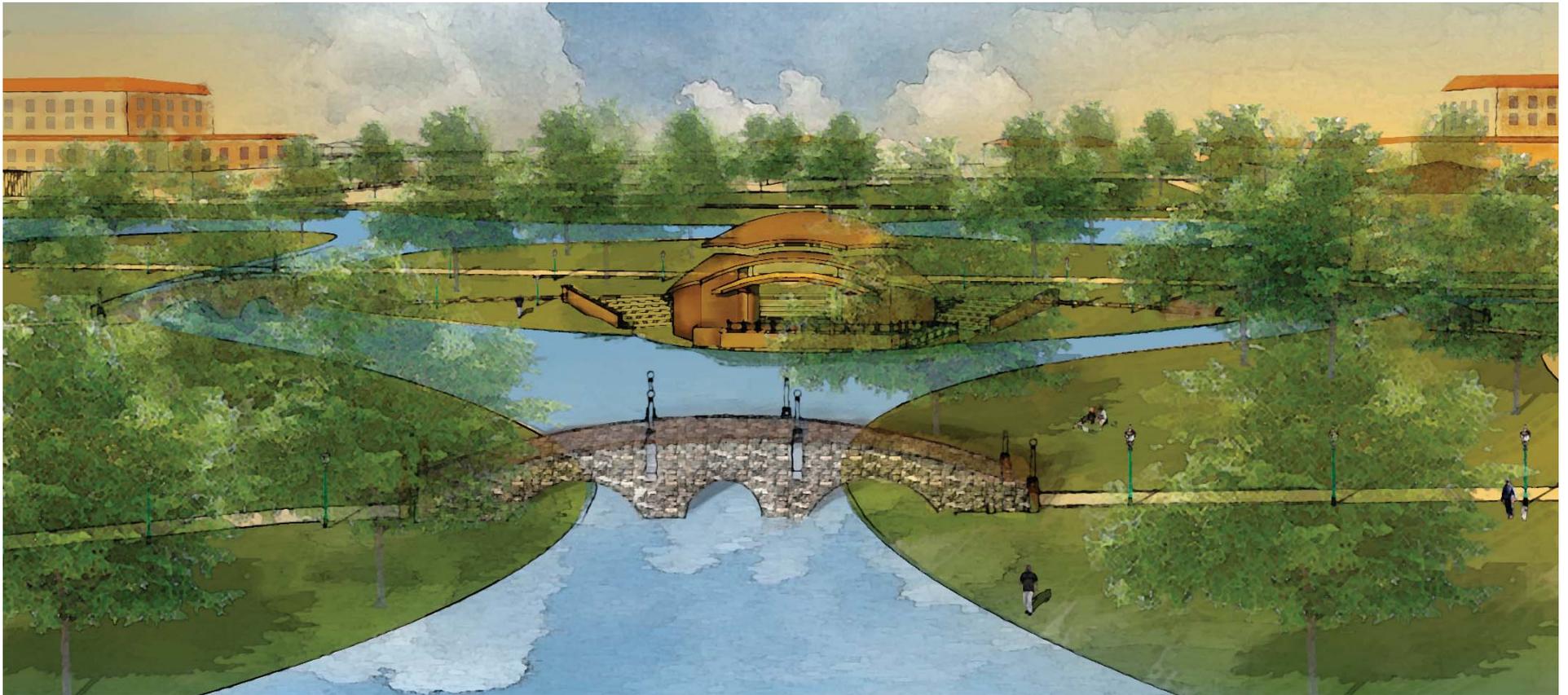
An essential component of the corridors urban concept plan will be the development of a hierarchy of streetscapes throughout the area. Streetscapes are the world of the pedestrian. They provide an attractive and pleasant quality of life that brings residents and consumers to any great and successful urban community. The components of streetscape are sidewalks, street trees, lighting, seating, bus stops, and street furnishings. These elements create pedestrian oriented and walk able environments. The streetscapes will provide connectivity through a network of streets, to all land use elements in the corridors. Shade is an important factor for the pedestrians to walk in Florida because of heat, humidity, and the long period of summer time. Street trees can provide necessary shade on the sidewalks as well as they function as buffer. The sidewalks are proposed on both sides along Tyndall Parkway and Highway 22 within CRA area. A system of secondary streetscapes will lead to and from the town center.



Typical streetscape shows sidewalks, planting areas, lighting, and street furnishings

Infrastructure Strategy

The biggest infrastructure component of the corridors concept plan is the creation of a state-of-the-art storm water management system. The plan proposes the creation of a new storm water treatment and collection system that will accommodate the vacant and redeveloped properties in the corridors area. The system will collect storm water runoff from the area and direct it into a series of treatment ponds in the town center. As a part of the pond construction process, excavated earth will be generated and used as fill to raise the pads for the future development in the corridors. This program will help clean the Bayou and mitigate the current flooding in the City.



Rendering of stormwater parks



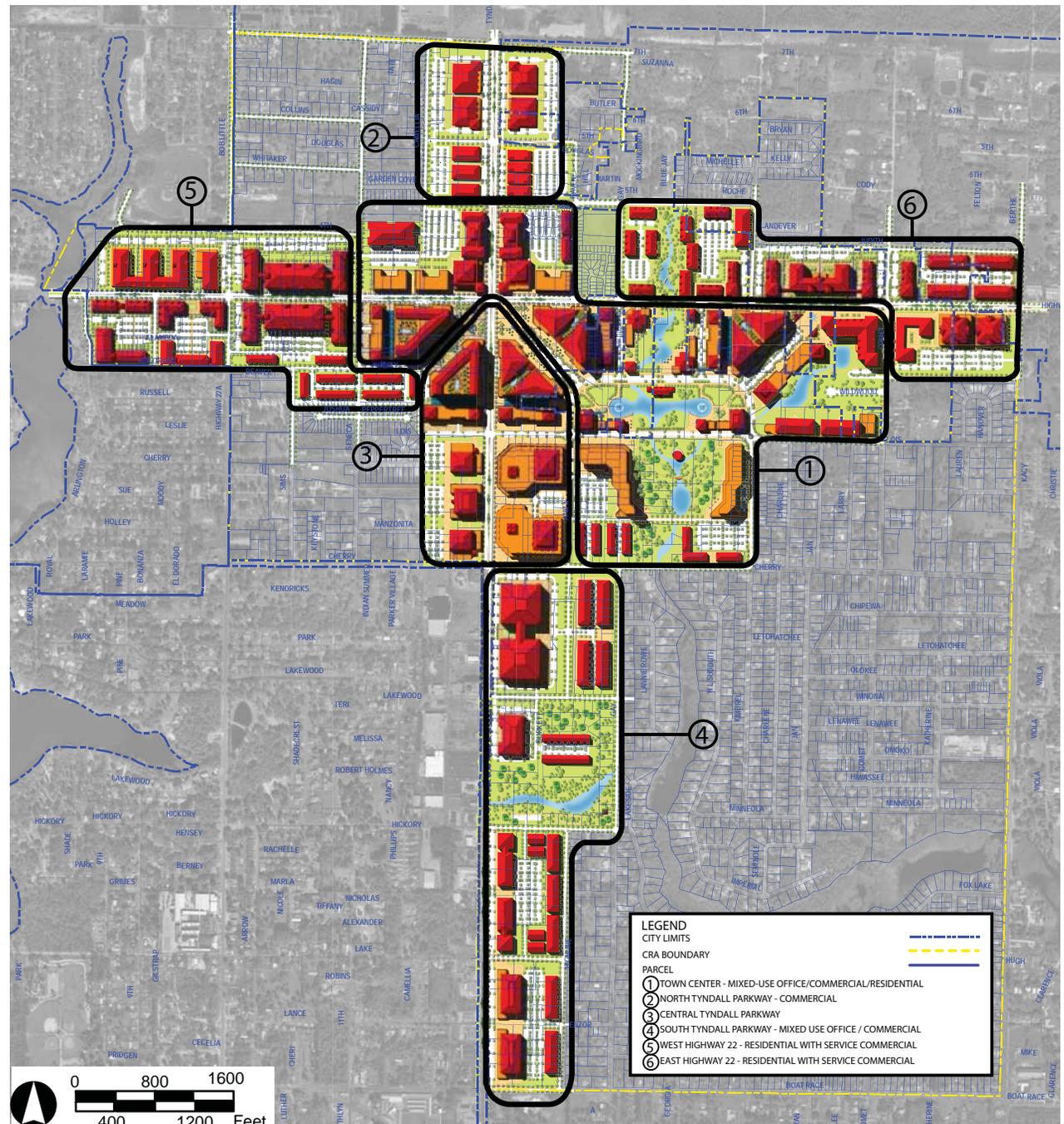
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THE CORRIDORS URBAN DESIGN PLAN

Overview

The City of Callaway “Central Corridors Urban Design Plan” provides a detailed description of the design intent for the future public and private redevelopment of the two major commercial corridors within the CRA and City of Callaway. The urban design plan and design guidelines have been created to provide a road map for phased development and redevelopment of these important corridors based upon the citizen’s collective vision for the future of their city. The plan is a very long range urban design vision. This long range vision assumes that all of the properties along both major corridors are redeveloped over time. The urban design plan is based upon the CRA redevelopment plan and as such, it focuses on creating a very strong community focus and an attractive town center that encourages residents and entrepreneurs to relocate in and invest in the Central Corridors of Callaway. This chapter is divided into three major sections. The first section describes the plan in total, the second focuses on the publicly owed lands and the important public sector improvements needed to stimulate the recovery of the corridors. The third section of this chapter focuses on the urban design character and configuration that is desired for the future private sector improvements in each of the six districts.

The Urban Design Plan



Urban Design Plan

The urban design plan is a physical form design plan that paints a picture of the 2 corridors once all of the public and private improvements have been made. This is a comprehensive design Vision that combines the public workshop “desired future” with the reality of the physical conditions of the corridor. The urban design plan is organized by the six main districts in the 2 corridors:

- District #1 – Town Center
- District #2 – North Tyndall Parkway
- District #3 – Central Tyndall Parkway
- District #4 – South Tyndall Parkway
- District #5 – West Highway 22
- District #6 – East Highway 22

Public Realm Improvements

Public realm improvement projects include: streetscape improvements, the creation of gateways and sidewalks, the creation of public plaza/ park, storm water treatments, roadway reconstruction, and infrastructure improvements within CRA area. The City/CRA initiates these projects to enhance the quality and function of the City in order to encourage private development activities.



Overall 3d build out plan, view from North West



Overall 3d build out plan, view from South East

Transportation

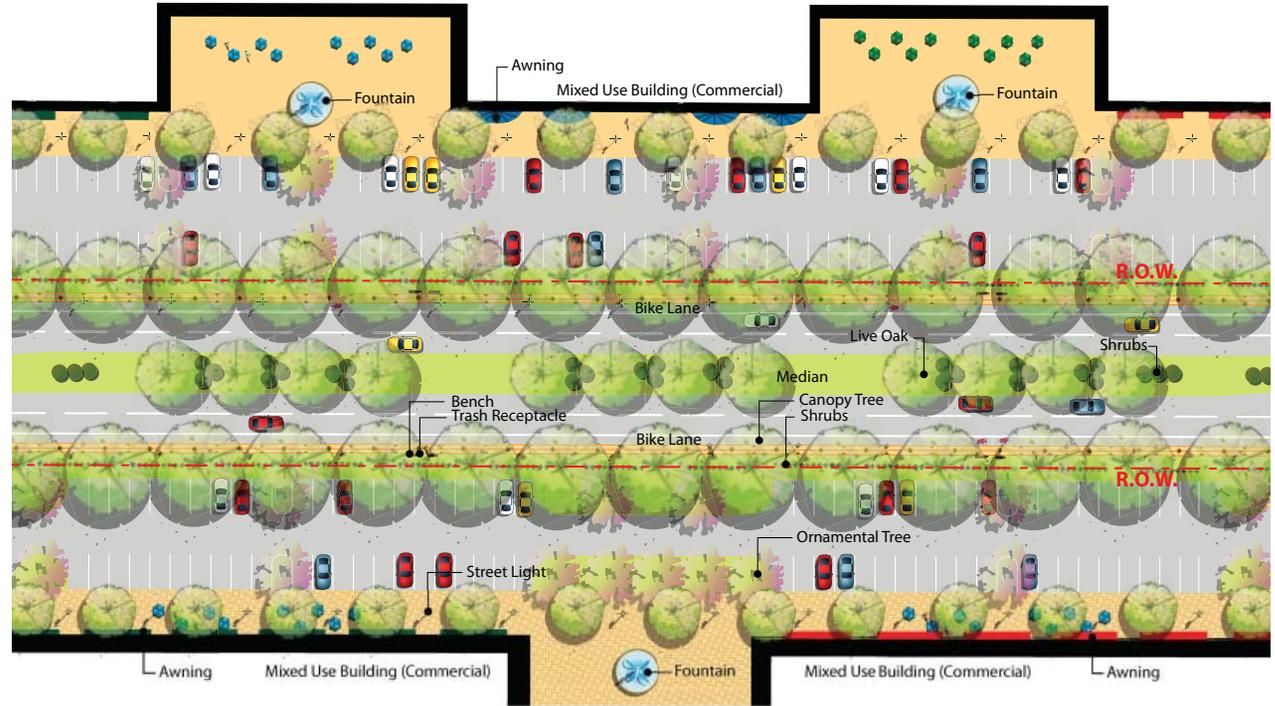
Tyndall Parkway

Tyndall Parkway is proposed to remain a 4-lane divided highway cross section throughout the CRA corridor. The current 12' lanes and 20' median will remain as they are currently. A series of roadway capacity and efficiency improvements are proposed for the roadway section as follows:

1. Close off any mid-block median cuts
2. Confine left-hand turns to the major intersections
3. Close all mid-block access points onto the Parkway
4. Create temporary and permanent "access-ways" for each block
5. Add on-street bikeways
6. Renovate and widen sidewalks

It will be necessary to coordinate these improvements with the City of Parker in order for the street to function as intended. The ROW in the southern and northern districts will have a landscape planting strip between the sidewalk and

the private parking areas. In the central district, the ROW will be one consistent pedestrian sidewalk environment straight to the building edges.



Tyndall Parkway typical street plan



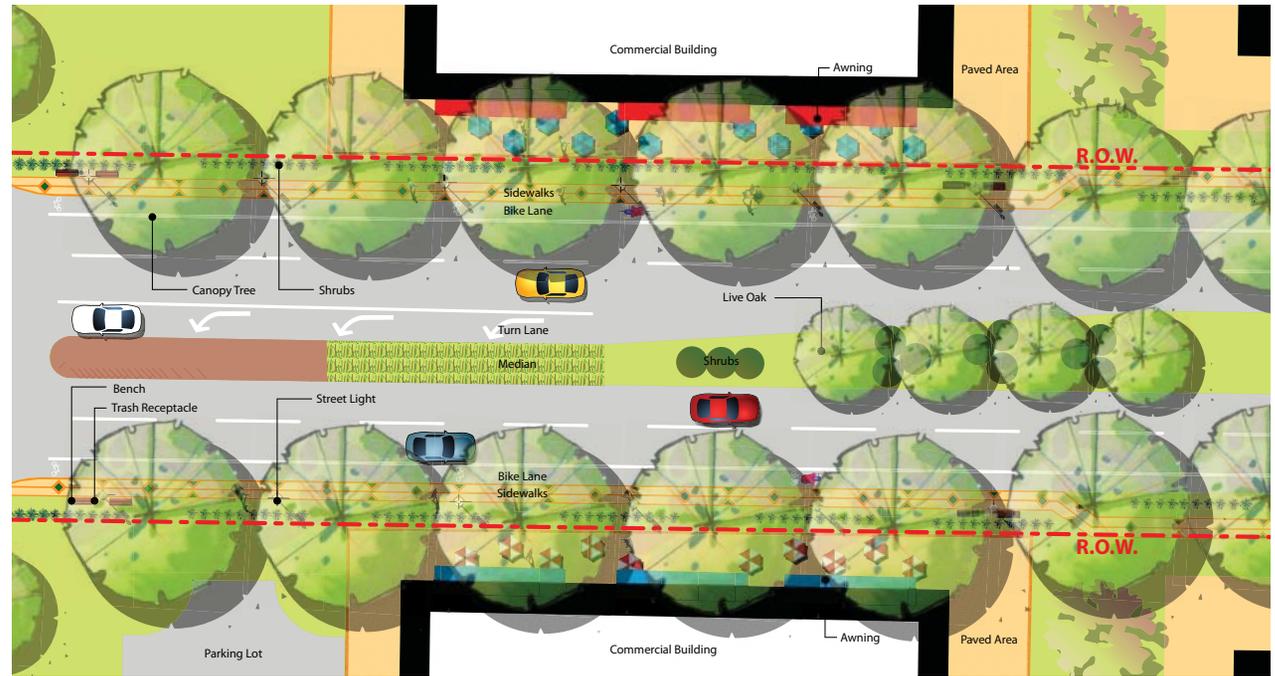
Tyndall Parkway typical street section

Highway 22

Highway 22 is proposed to be reconstructed to a 4-lane divided highway cross section throughout the CRA corridor. This reconstruction will be done in concert with the FDOT and their proposed Gulf Coast Parkway plan. The new roadway cross-section will have 11' lanes with a 22' median. In addition, there will be the following design elements as a part of the preliminary plan presented to the FDOT for implementation:

1. There will be no mid-block median cuts
2. Left-hand turns will be confined to the major intersections
3. There will be no direct access to Highway 22 from individual parcels
4. All access to private property will be from side-streets and an alleyway system that will run parallel to Highway 22 and behind the proposed buildings
5. There will be on-street bikeways
6. Handicap accessible ROW sidewalks
7. Median "pedestrian safe zones" at cross-walk locations
8. Accommodations for "off-road" transit stops

use is proposed for residential use. At the corners, where commercial use is proposed, the ROW will be one consistent pedestrian sidewalk environment straight to the building edges.



Highway 22 typical street plan

The ROW along the corridor will have a landscape planting strip between the roadway and the sidewalk and the sidewalk and the buildings where the land



Highway 22 typical street section

Parking

Side-street Parking

The main roadways through the corridors will be designed for a 35 mph design speed. This design speed and the roadway function classifications will not be able accommodate on-street parking on these 2 main arterials. However, the design plan does propose on-street public parking on the side-streets entering the main roadways and in the town center. The side-streets will be designed for 25 mph. The plan has diagonal parking on the side-streets that intersect with the main thoroughfares and provide entry to the “access-ways” for the commercial frontage. These diagonal parking spaces will also provide additional / convenient parking for the commercial businesses that face the side-streets.



Town-Center Parking

The town-center streets will be designed for 10 mph. As such, these streets can easily accommodate on-street parking. It will be essential to have on-street parking in the town center to provide convenient access to the civic and retail businesses on the ground floors. The design plan has diagonal and 90-degree parking proposed for all of the streets in the town center.





Rendering of town center parking and streetscape

Gateways

Corridor Gateways

There are 4 proposed “Corridor Gateways”. These gateways are located at the entrances to the commercial corridors along Highway 22 and Tyndall Parkway. The Tyndall Parkway gateways will be at Boat Race Road and at 7th Street. The Highway 22 gateways will be at the Bayou Park gateway at Arlington Street and at the eastern entrance at Comet Avenue. These gateways will be monument signage that signifies the advent of the Callaway shopping district. The design style will incorporate the North Florida brick and wrought iron vernacular.



Town Center Gateways

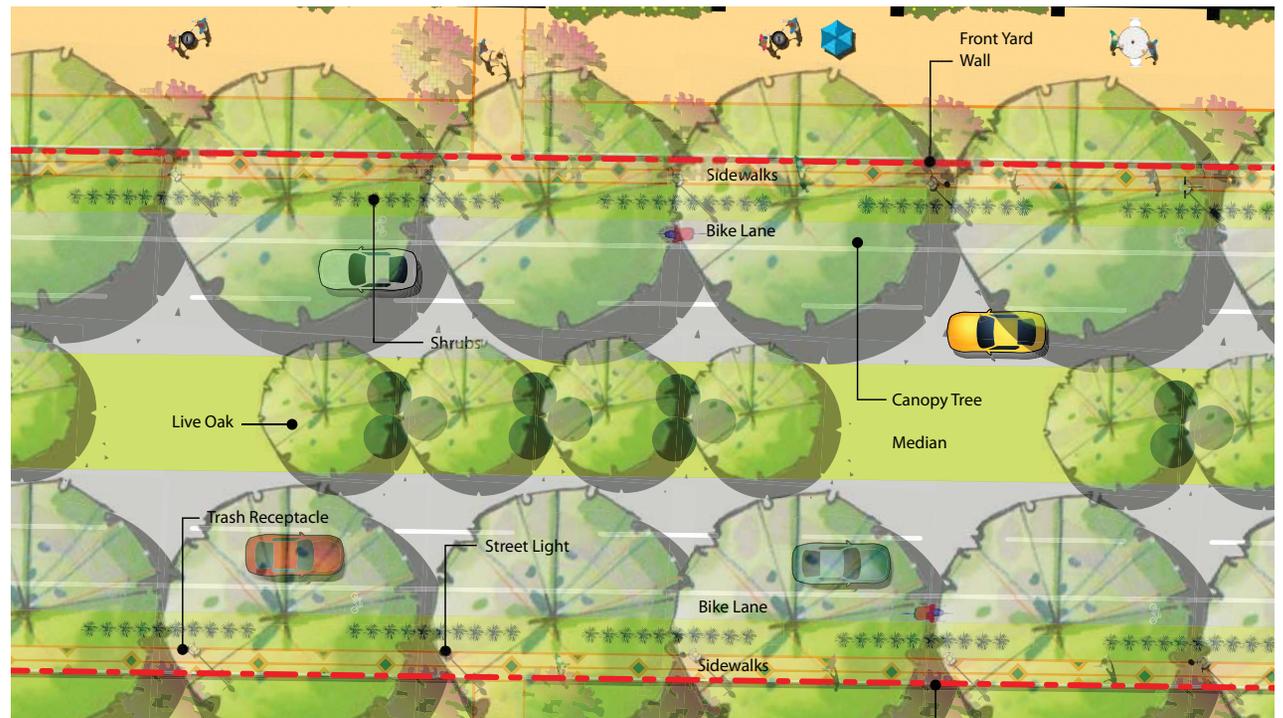
There are 3 proposed “Town-Center Gateways”. These gateways are located at the edges of the town-center development adjacent to Highway 22 and Tyndall Parkway. One will be at the intersection of Tyndall Parkway and Highway 22, one will be on Highway 22 between Comet and Gay, and one will be on Tyndall Parkway at Cherry Street. These gateways will be pylon signage that signifies the entrance to the Town-Center of the Callaway shopping district. The design style will incorporate the North Florida brick and wrought iron vernacular.



Streetscape

Tyndall Parkway and Highway 22 Median Landscape Design

The medians for both of the major roadways will have a planting palette of native landscape materials. The dominant canopy tree could be the Live Oak or similar to native shade trees and will be located in the center in an equally spaced organization with a central cluster on every median block. Under-story plantings will be arranged along the middle of the medians and be composed of hardy and native materials; Indian Hawthorn, Coontie, Liriope, Holly, Photinia, Firebush, Dwarf wax-myrtle, Bird of paradise, and Sword fern.



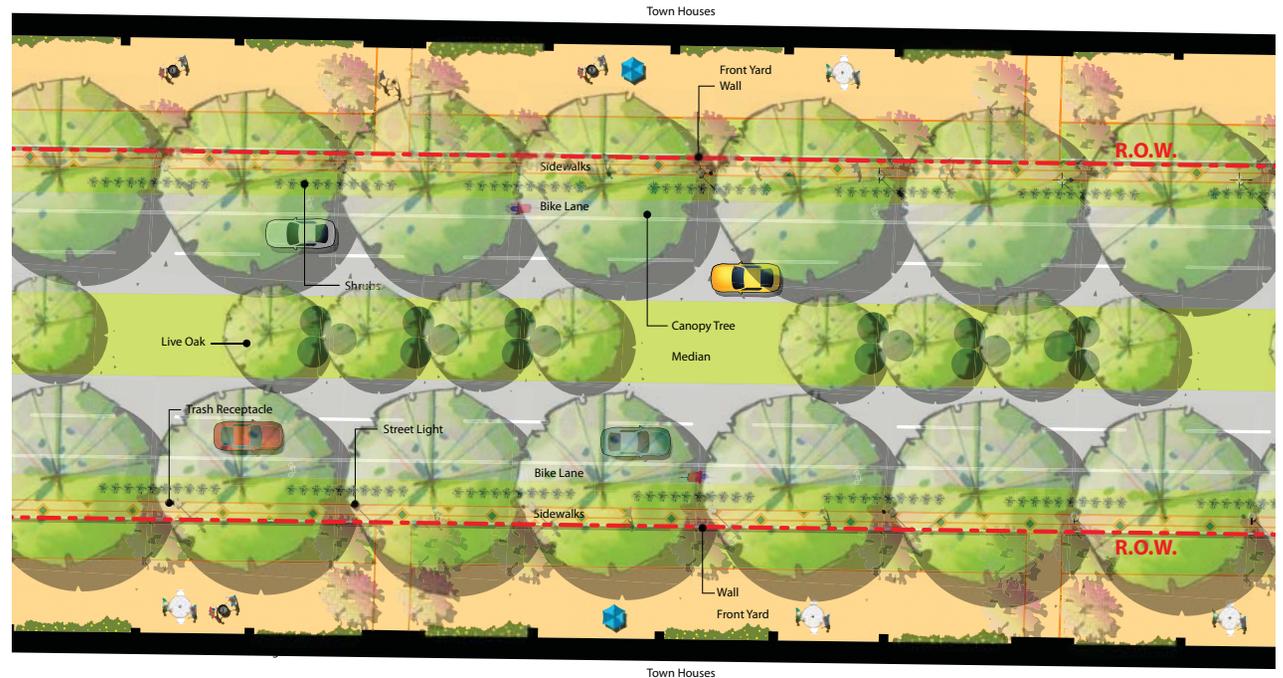
Typical median plan



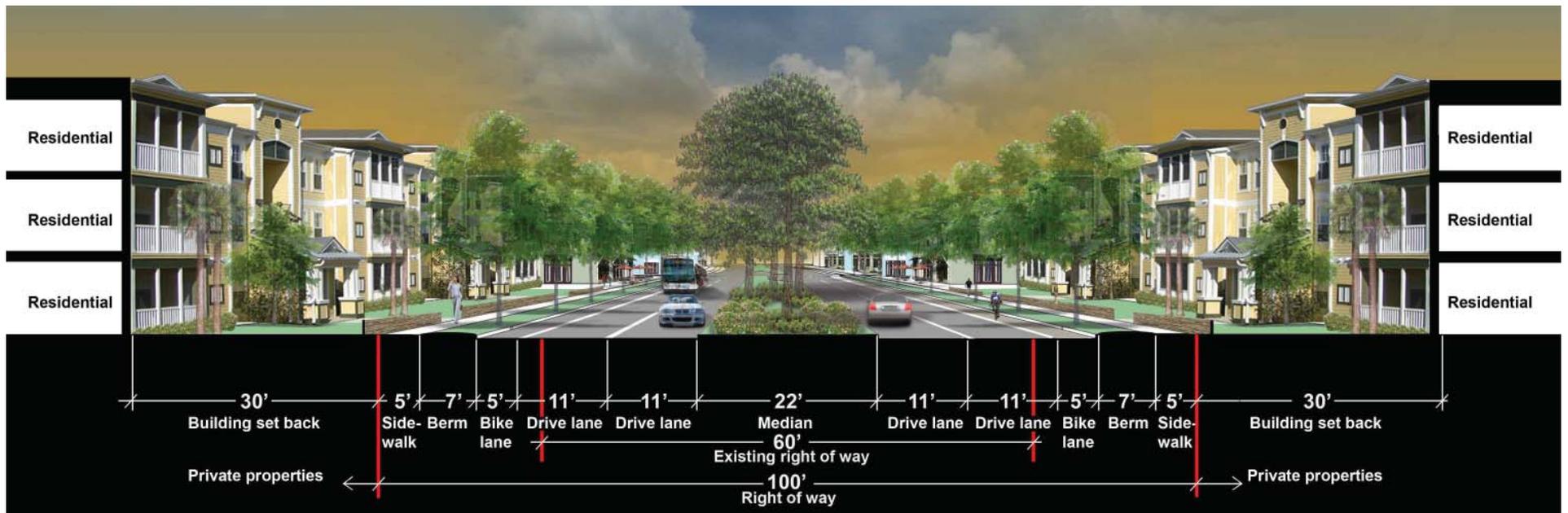
Typical median section

Highway 22 – Residential ROW

The streetscape design for the areas adjacent to multi-family residential projects on Highway 22 will incorporate a soft-scape design theme to create a traditional boulevard landscape setting. The sidewalks will be set 7 feet off of the roadway and separated by a grass landscape strip. On the private side of the sidewalk will be a lawn that continues to the building edge. At building entrances, there will be connecting sidewalks that extend out to the roadway sidewalk. The public/private edge will be demarcated by a low landscape wall and plantings. There will be a row of canopy-scale Live Oak Street trees or similar to native shade trees running parallel to Highway 22. These will be in the 7' landscape strip midway between the 5' sidewalk and the roadway



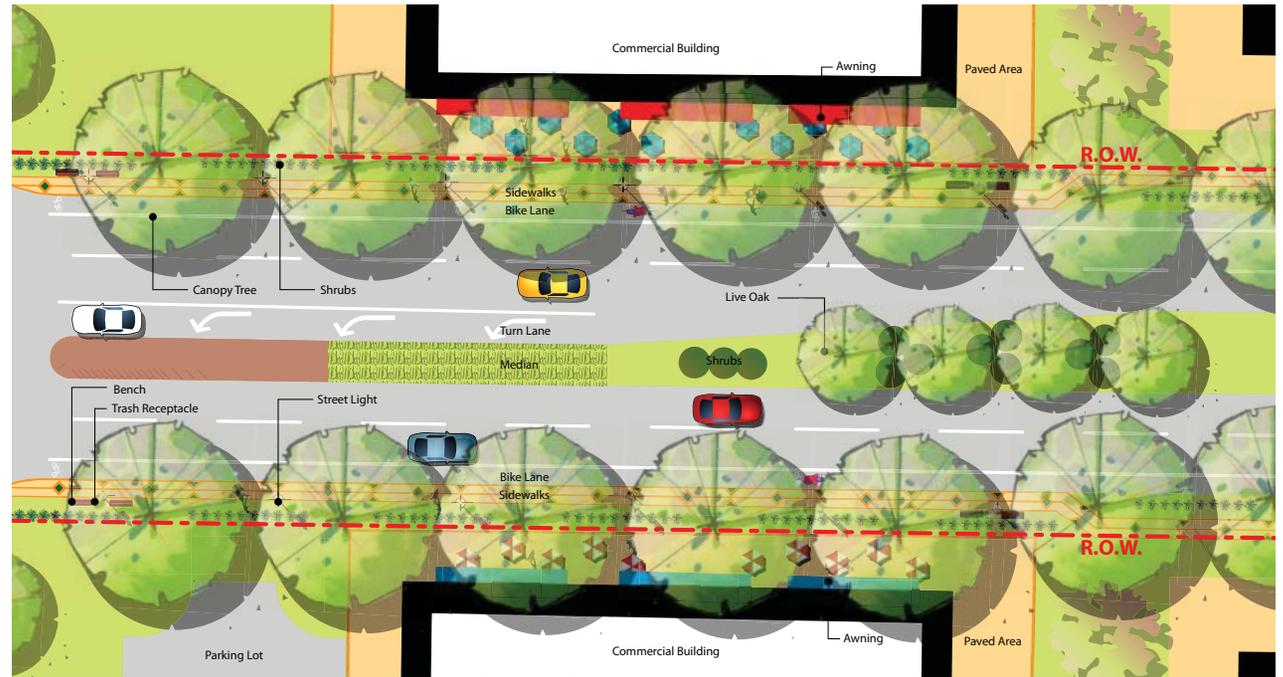
Highway 22 - Residential, Streetscape Plan



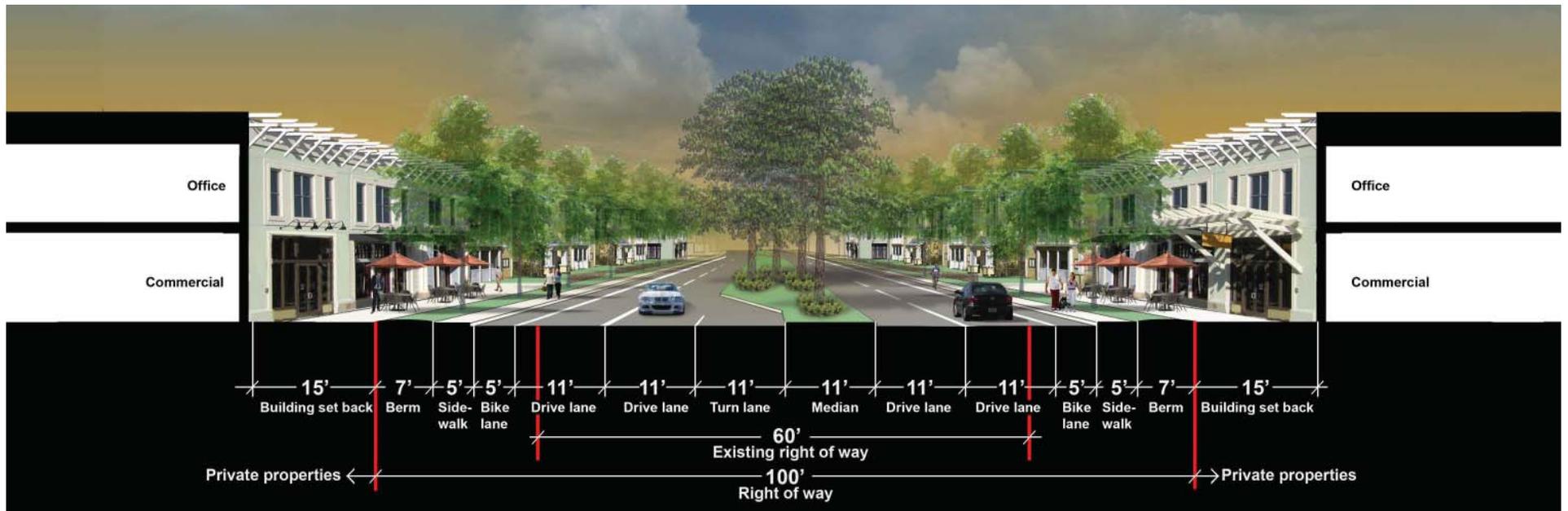
Highway 22 - Residential, Streetscape Section

Highway 22 – Commercial ROW

The streetscape design for the areas adjacent to mixed-use commercial projects on Highway 22 will incorporate a hard-scape design theme to create a neighborhood store setting. The sidewalks will be continuous between the roadway curb and the storefront. These areas will be designed as pedestrian plaza spaces and will have outdoor cafes and seating areas. These plazas will have canopy Live Oak trees or similar to native shade trees also; however, they will set in seat-wall planters at the ROW edge.



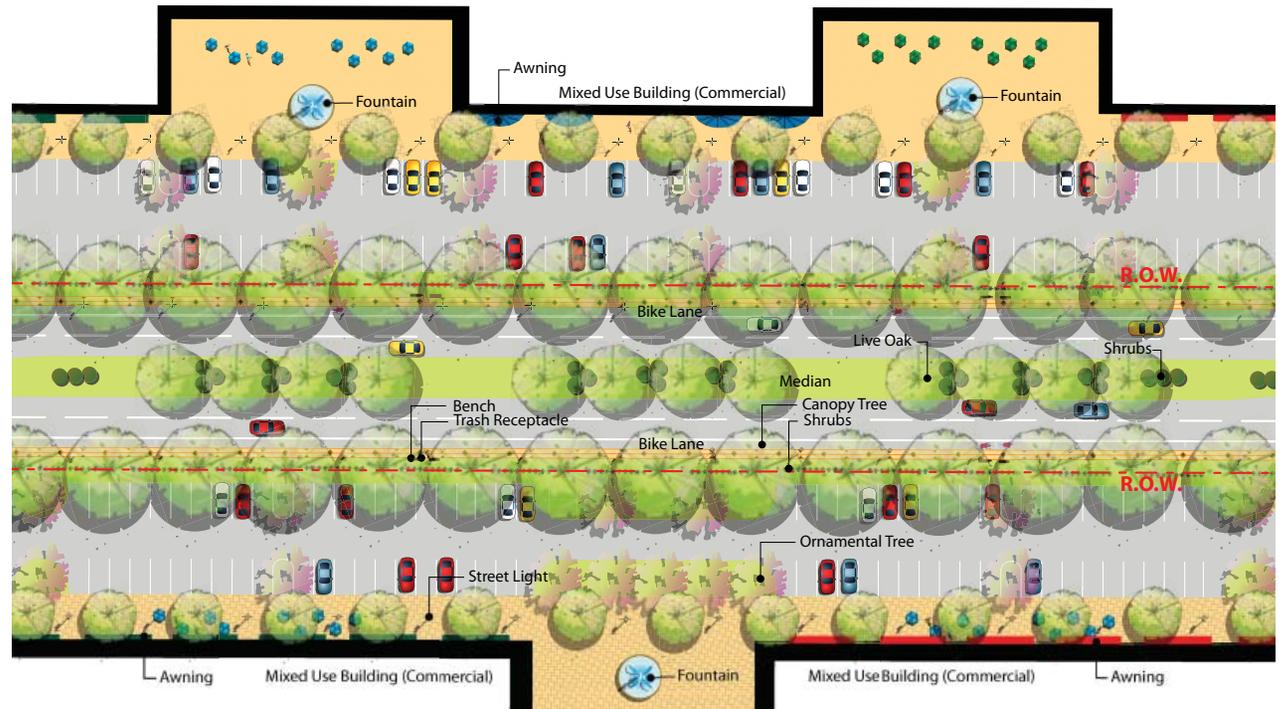
Highway 22 - Commercial, Streetscape Plan



Highway 22 - Commercial, Streetscape Section

Tyndall Parkway – Commercial / Office ROW (Between Cherry and Boat Race)

The high-speed and noisy traffic of Tyndall Parkway is not conducive to a quality pedestrian environment at street edge. Where Tyndall Parkway is adjacent to the proposed mixed-use development of office/commercial buildings, this is the case. For this reason, it is proposed that there be a parking bay/access-way separating the building edge from the roadway. The pedestrian spaces will be along the building frontages and on the side-street areas where there is less auto-related impacts to the pedestrian. In these instances, the public right of way will have a small subtle berm that separates the parking bay from the Tyndall sidewalk environment. Along this berm will be a series of Live Oak street trees or similar to native shade trees planted parallel to Tyndall Parkway with 50' spacing.



Tyndall Parkway - Commercial / Office ROW, Streetscape Plan



Tyndall Parkway - Commercial / Office ROW, Streetscape Section

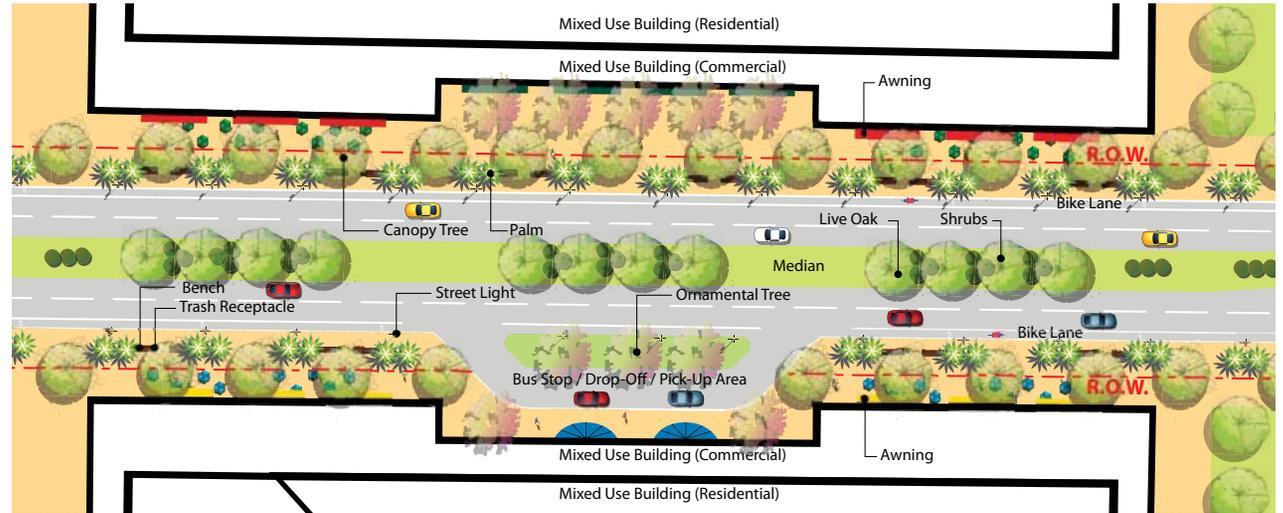
Tyndall Parkway – Town Center ROW (Between Highway 22 and Lois)

The high-speed traffic of Tyndall Parkway will slow down as it approaches the intersection of Highway 22 due to the posted speed limit signage and imminent traffic signal. It is in this section of the Parkway that the road is adjacent to the town center. For this reason, it is proposed that the mixed-use buildings be closer to the roadway and have a streetscape pedestrian relationship with the Parkway. This section of the roadway will also have the major town-center transit stop on both sides of Tyndall Parkway. In this section, the streetscape will be a continuous hard-scape plaza from the Parkway to the building edges. Along this streetscape will be a series of double Washingtonia Palm Street-trees planted parallel to Tyndall Parkway with a 40' spacing in tree-grates and tree guards.

Town Center Streetscape

There will be a variety of sidewalk widths and resulting streetscape designs in the town center

area. For the most part, these streetscapes will be entirely paved sidewalks from curb to storefront. The typical width will be 12'. The town center streetscapes will have Sable Palm street trees on 24' spacing. At street corners and mid-block plaza areas, there should be Live Oak canopy trees or similar to native shade trees for shade.



Tyndall Parkway - Town Center ROW, Streetscape Plan



Tyndall Parkway - Town Center ROW, Streetscape Section

Street Furnishings

The future streetscape and pedestrian environments of the commercial corridor and town center will need to have a sense of continuity of design to “tie the area together”. This is accomplished by having a family of paving patterns and materials, a family of common street trees, and a family of similar street furnishings. The Callaway corridors street furnishings theme will again echo the North Florida brick, limestone, and wrought iron palette. The family of street furnishings contains: tree grates, tree guards, benches, trash receptacles, pedestrian lights, bollards, planters, and kiosks.



Bollards and planters



Trash receptacle



Pedestrian lights



Pedestrian orientation kiosk



Benches

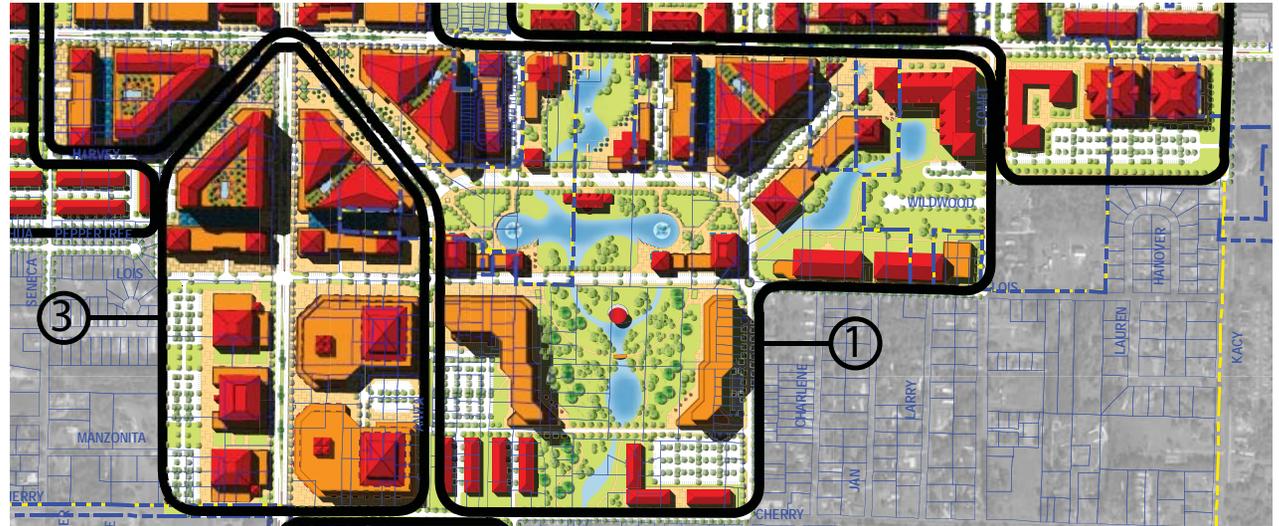


Tree Grate

Public Parks

There are three major public parks that are a part of the urban design plan. One is a gateway park at the western entrance to the corridor on Highway 22. Another park is located on Tyndall Parkway at Hickory, and the last one is a series of park spaces that compose the town commons for the new town center. The Gateway park is intended to be a passive park that relates to the beauty of the Bayou tributary. The park will have picnic facilities, walking trails, playground, boardwalks and open field play areas. This green space will help signify the beginning of the commercial corridor. Hickory park will be designed as neighborhood park for the surrounding multi-family residents and the daytime businesses as well. The park is located along an existing drainage that is tributary to the Callaway Bayou. The land is currently vacant and can be purchased and developed as an urban run-off treatment filter for the Tyndall Parkway run-off. The park should contain picnic facilities, jogging / exercise trails, basketball and tennis courts, and playground facilities. The town center commons will be a series of storm water treatment ponds that are the focal point of the park. The commons will have three separate areas. North commons will be set in the middle of a predominantly retail shopping area. The open space will provide an elegant foreground for the outdoor café plaza spaces that front on them. The Middle commons will be the focal piece for the civic square. The park will be set in the center of a series of public spaces that are oriented towards the central lake with jetted fountains. There will be a promenade that goes around the lake and links the City buildings together with a sequence of outdoor plazas. The park could feature formal gardens, sculpture gardens and a central farmer's market space. The southern commons will be a wide open free play area or great lawn space for events and festivals of all sorts. The centerpiece of the southern commons will be the outdoor amphitheater for weekend performances. On the edges of the park will be two major mixed-use residential developments. The residents will have incredible

views into the park spaces. On the ground floor will be retail venues and a series of restaurants with outdoor terraces facing the center of the park and amphitheater.



The town center commons plan



The town center commons rendering

Signage

The corridor commercial area will have its own “Way-finding” system to assist all the visitors and residents alike to find their way around the town center and adjacent corridors. There will be 2 types of signage; identification signs and directional signs. The design style for the signs will match that of the street furnishings: the North Florida brick, limestone, and wrought iron palette.



Pedestrian signage

Vehicular signage

Site information

Private Realm Development

The private realm along the corridors consists of the private land and buildings that are adjacent to the main thoroughfares in the City. The primary premise of this urban design plan is that eventually, all of these properties will reach a point in their economic lives where it becomes profitable to redevelop them. When this point occurs, then the redevelopers will look to the city codes to decide what options they have and in what fashion they may proceed. In order to be prepared for this phase of the evolution of the corridor, there must be an urban design scheme in place that can be translated into a code to guide these redevelopment efforts into one common vision. The following is a description of the proposed attributes for the redevelopment of these private land holdings in the corridor. It is these guidelines that would be used to start the process of creating a zoning overlay district for the corridor.

District #1- Town Center



District #1- Town Center

These are recommendations that require changing the LDR and zoning code in order to affect the proposed urban design intent.

Land Use

- Mixed-Use; residential, commercial office, institutional
 - Condominiums, corporate office, professional office, services office, apparel, consumer goods, service retail, restaurant

Height / Mass

- 6-story, 1-commercial, 1-office, 4-residential
- 500' maximum façade length
- 100' façade breaks
- 1st. and 2nd. floor vertical set-back

Architecture

- Contemporary / modern
 - North Florida Brick Georgian, Brick Victorian Storefront, New Orleans, and Key West

Set-backs

- Front - 10'
- Sides - 10'
- Rear - 10'

Density

- FAR = 5:1

Lot Size

- Front line - 200'
- Side line - 200'
- 40,000 square feet minimum

Parking

- 1 parking space for 300 square feet
- Internal structured parking

Landscape

- Street-front minimums

Pedestrian

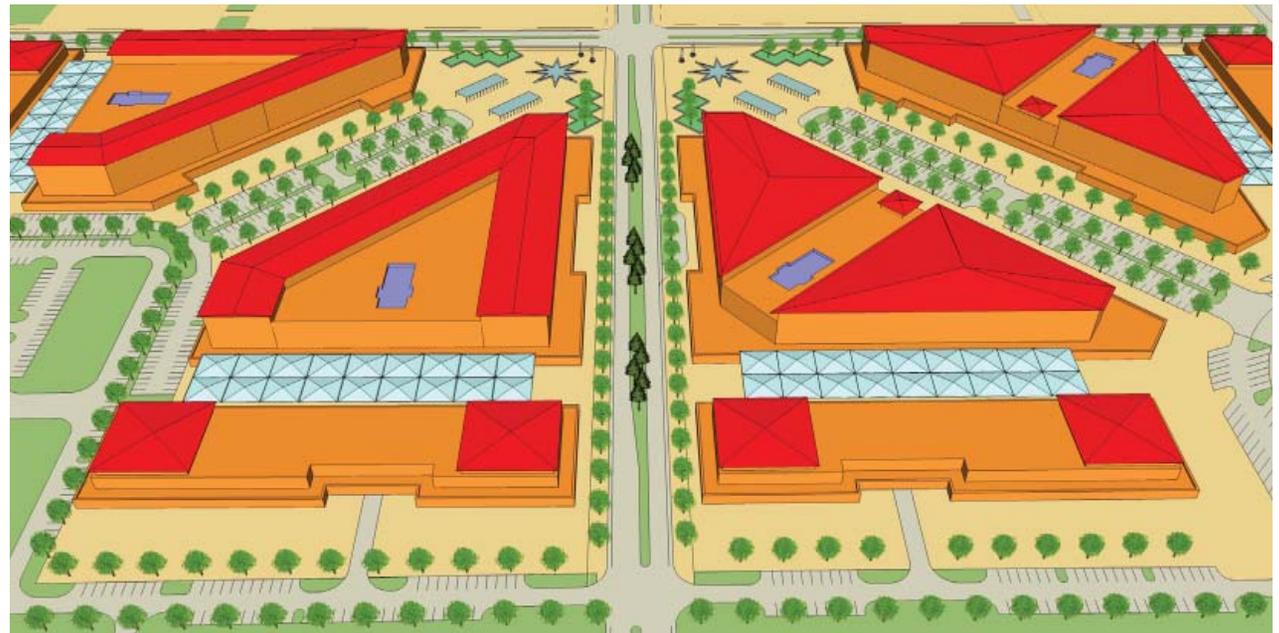
- Entry plazas
- Sidewalk minimums
- ADA accessibility
- Required furnishings
- Café set-backs



Typical mixed-use; outdoor cafe



Typical mixed-use; residential above commercial / office



District 3d diagram



Conceptual elevations

District #2 – North Tyndall Parkway



District #2 – North Tyndall Parkway

These are recommendations that require changing the LDR and zoning code in order to affect the proposed urban design intent.

Land Use

- Commercial on the Tyndall Parkway frontage
 - Household products, electronics, etc.
- Multi-family residential on the back half of the block

Height / Mass

- 2-story
 - 400' maximum façade length
 - 100' façade breaks

Architecture

- Contemporary / modern
 - Brick Victorian Commercial

Set-backs

- Front - 80'
- Sides – 0', 12' on the corners
- Rear – 12'

Density

- FAR 1:1

Lot Size

- Front line - 150'
- Side line – 200'
- 30,000 square feet minimum

Parking

- 1 parking space for 300 square feet
- 1 bay required front parking
- Remainder behind building

Landscape

- Parking lot minimums
- Store-frontage minimums
- Street-front minimums

Pedestrian

- Entry plazas
- Sidewalk minimums
- ADA accessibility
- Required furnishings



Typical multi-family residential



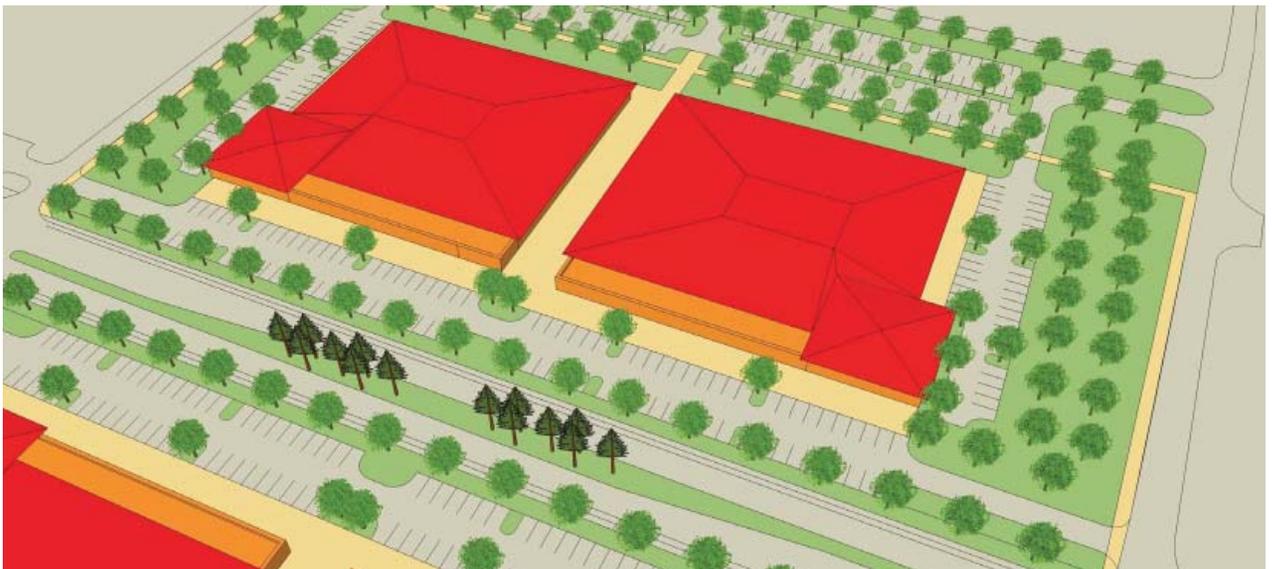
Typical "big box" commercial



Typical multi-family residential

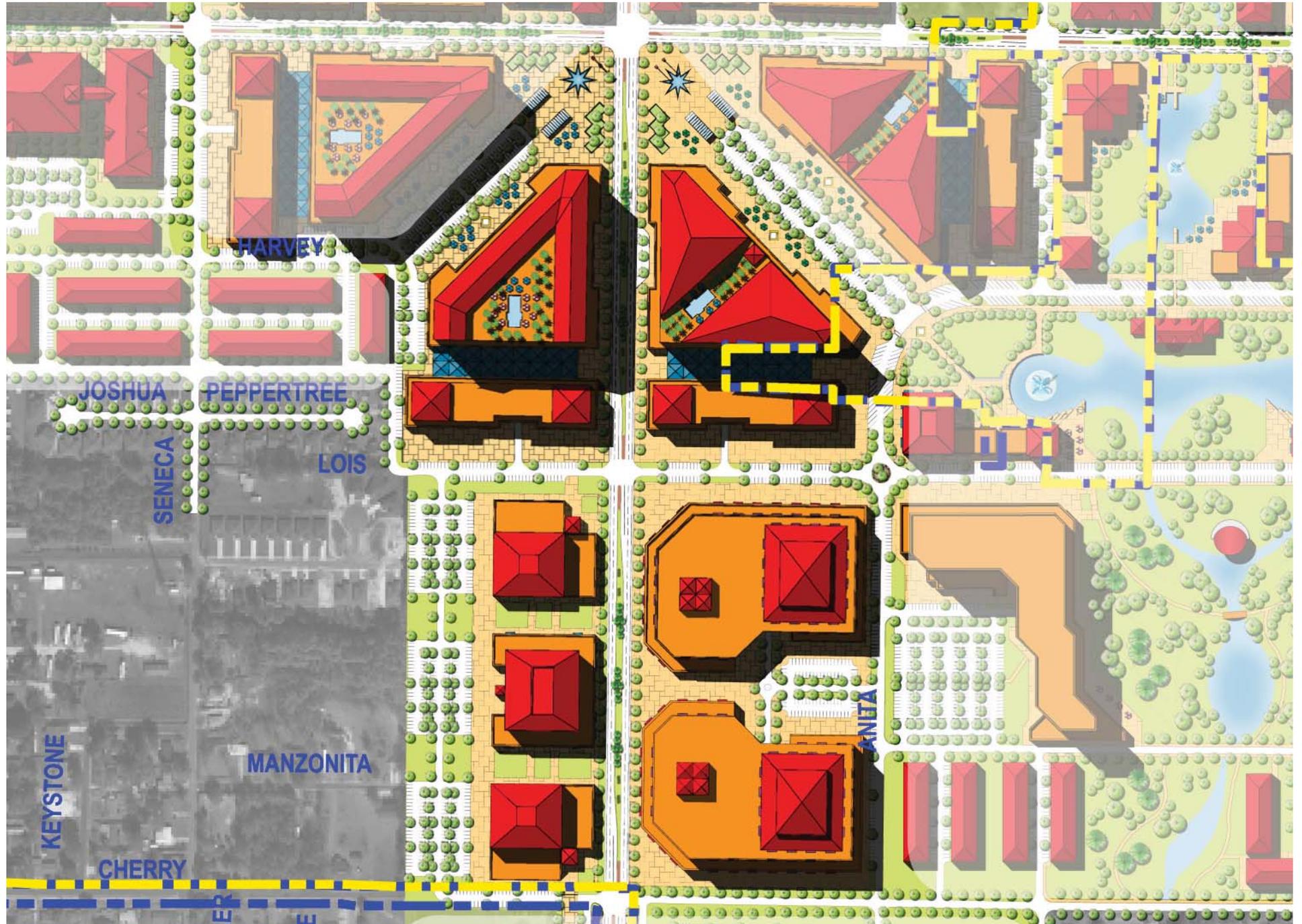


Typical "big box" commercial



District 3d diagram

District #3 – Central Tyndall Parkway



District #3 – Central Tyndall Parkway

These are recommendations that require changing the LDR and zoning code in order to affect the proposed urban design intent.

Land Use

- Mixed-Use; residential, commercial office, institutional
 - Condominiums, corporate office, professional office, services office, apparel, consumer goods, service retail, restaurant

Height / Mass

- 6-story, 1-commercial, 1-office, 4-residential
- 500' maximum façade length
- 100' façade breaks
- 1st. and 2nd. floor vertical set-back

Architecture

- Contemporary / modern
 - North Florida Brick Georgian, Brick Victorian Storefront, New Orleans, and Key West

Set-backs

- Front - 10'
- Sides – 10'
- Rear – 10'

Density

- FAR = 5:1

Lot Size

- Front line - 200'
- Side line – 200'
- 40,000 square feet Minimum

Parking

- 1 parking space for 300 square feet
- Internal structured parking

Landscape

- Street-front minimums

Pedestrian

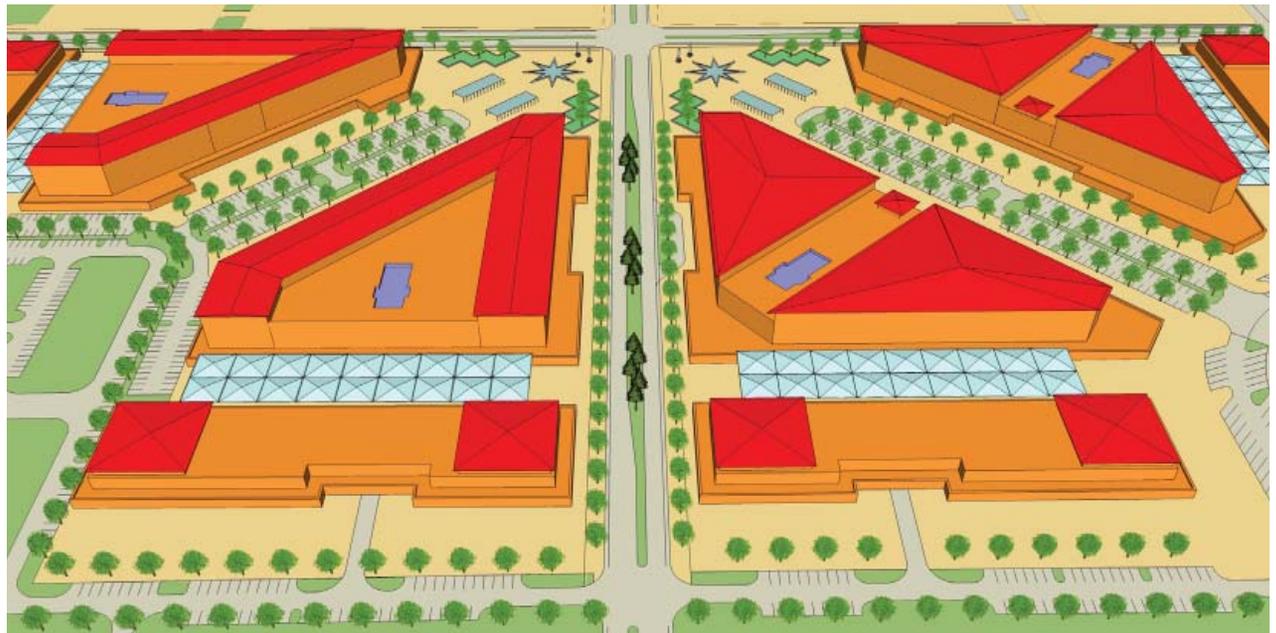
- Entry plazas
- Sidewalk minimums
- ADA accessibility
- Required furnishings
- Café set-backs



Typical mixed-use; outdoor cafe



Typical mixed-use; residential above commercial / office



District 3d diagram



Conceptual elevations

District #4 – South Tyndall Parkway



District #4 – South Tyndall Parkway

These are recommendations that require changing the LDR and zoning code in order to affect the proposed urban design intent.

Land Use

- Mixed-Use; Commercial / office on Tyndall Parkway frontage
 - Corporate office, professional, real estate / insurance services, banking and finance, commercial goods and services.
- Multi-Family Residential – Highline and Gay frontage
 - Apartments and condominiums

Height / Mass

- Mixed-use: 3-story, 1-commercial, 2-office
 - 300' maximum façade length
 - 100' façade breaks
 - 1st. floor vertical set-back
- Residential: 3-story
 - 300' maximum façade length
 - 100' façade breaks

Architecture

- Contemporary / modern
 - North Florida Brick Georgian, Brick Victorian Storefront, New Orleans, and Key West

Set-backs

- Mixed-use
 - Front - 80'
 - Sides – 0', 12' on the corners
 - Rear – N/A
- Residential
 - Front - 15'
 - Sides – 15' on the corners
 - Rear – N/A

Density

- FAR = 2:1

Lot Size

- Front line - 100'
- Side line – 200'
- 20,000 square feet minimum

Parking

- Mixed-use:

- 1 parking space for 300 square feet
- 1 bay required front parking
- Remainder behind building
- Residential:
 - 2 spaces per unit
 - Underneath or behind building

Landscape

- Parking lot minimums
- Store-frontage minimums
- Street-front minimums

Pedestrian

- Entry plazas
- Sidewalk minimums
- ADA accessibility



Typical mixed-use; 1-commercial, 2-office

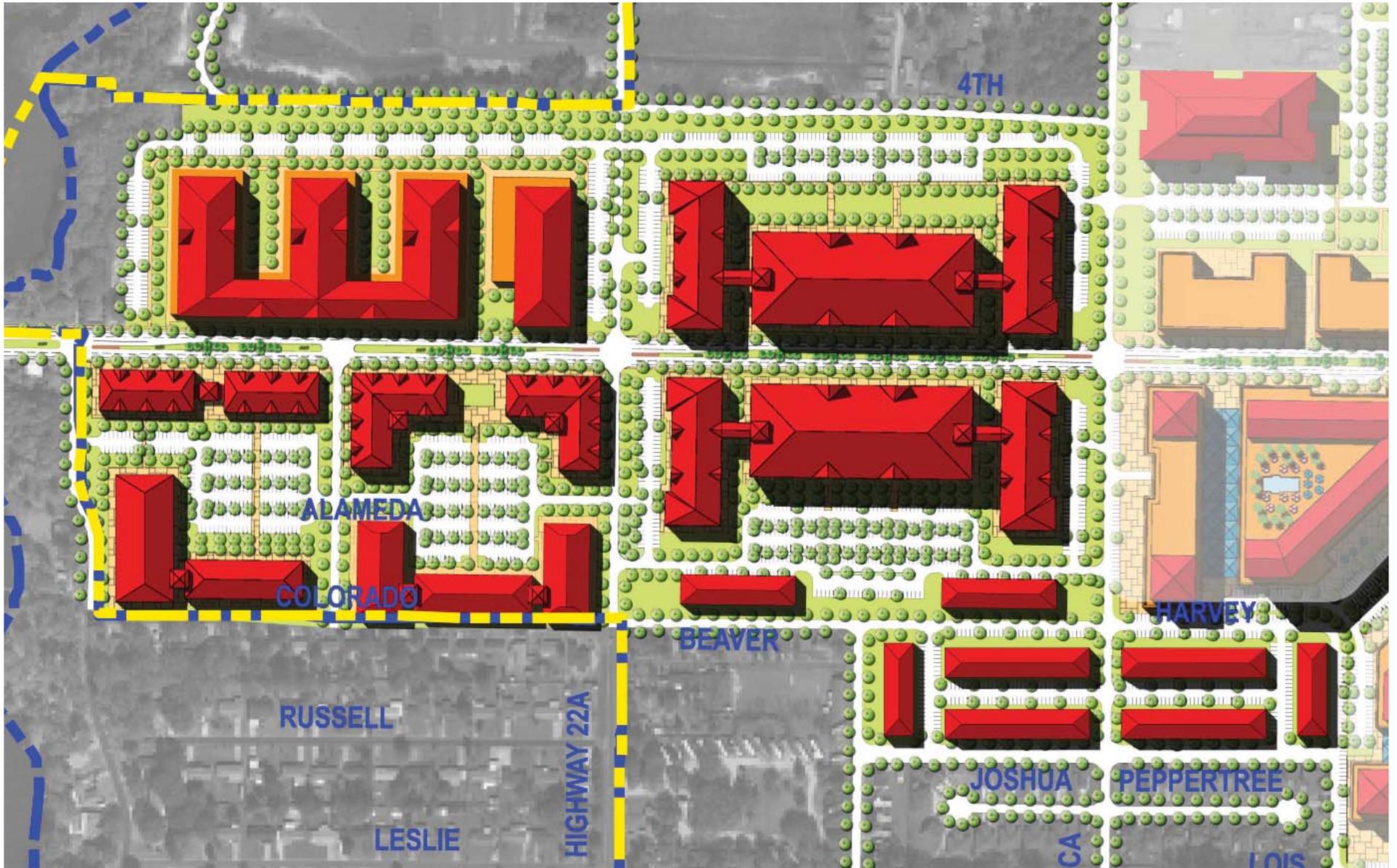


Typical office



District 3d diagram

District #5 – West Highway 22



District #5 – West Highway 22

These are recommendations that require changing the LDR and zoning code in order to affect the proposed urban design intent.

Land Use

- Mixed-Use; Commercial / Multi-family
 - Convenience retail, restaurant, lounge
 - Apartment / condominium
- Multi-Family Residential
 - Apartments and condominiums

Height / Mass

- Mixed-use: 3-story, 1-commercial, 2-residential
 - 300' maximum façade length
 - 100' façade breaks
 - 1st. floor vertical set-back
- Residential: 3-story
 - 300' maximum façade length
 - 100' façade breaks

Architecture

- Contemporary / modern
 - North Florida Brick Georgian, Brick Victorian Storefront, New Orleans, and Key West

Set-backs

- Mixed –use
 - Front - 10'
 - Sides – 80' street side, 0 other side
 - Rear – 80'
- Residential
 - Front - 30'
 - Sides – 0', 12' on the corners
 - Rear – 80'

Density

- Mixed – Use: FAR = 2:1
- Residential: 2:1

Lot Size

- Mixed – Use
 - Front line - 100'
 - Side line – 200'
 - 20,000 square feet minimum
- Residential
 - Front line - 200'
 - Side line – 200'

- 40,000 square feet minimum

Parking

- Mixed-use:
 - Commercial; 1 parking space for 300 square feet
 - Residential: 2 space per unit
 - 1 bay required corner side yard parking
 - Remainder behind building
- Residential:
 - 2 spaces per unit
 - Underneath or behind building

Landscape

- Parking lot minimums
- Store-frontage minimums
- Street-front minimums

Pedestrian

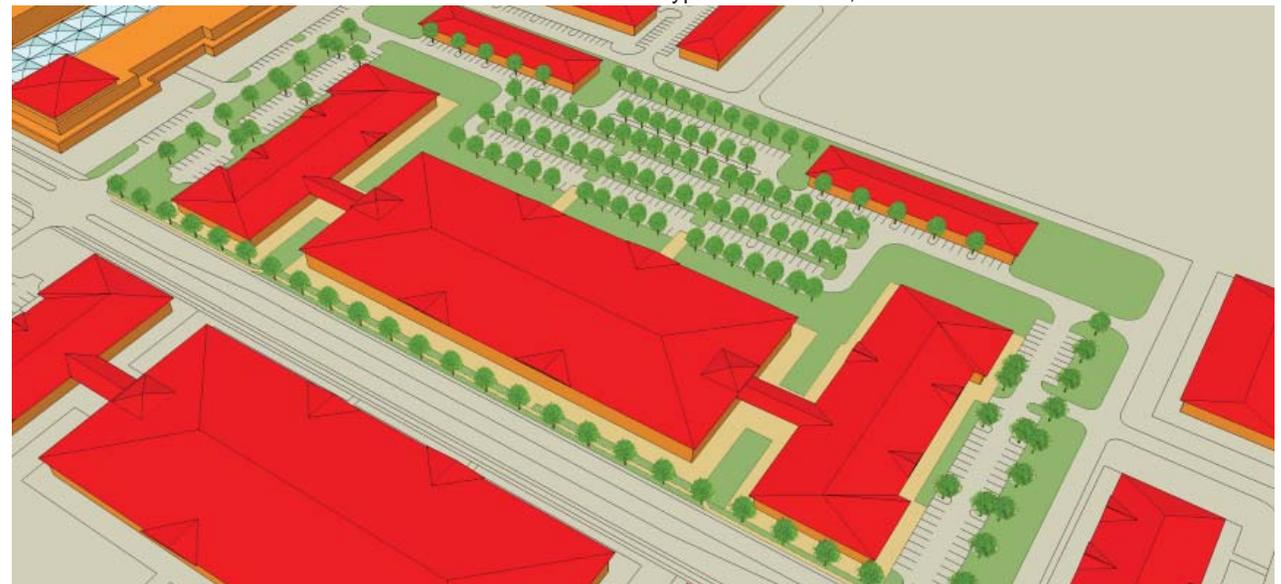
- Entry plazas
- Sidewalk minimums
- ADA accessibility



Typical multi-family residential



Typical mixed-use; 1-commercial and 1-residential



District 3d diagram

District #6 – East Highway 22



District #6 – East Highway 22

These are recommendations that require changing the LDR and zoning code in order to affect the proposed urban design intent.

Land Use

- Mixed-Use; Commercial / Multi-family
 - Convenience retail, restaurant, lounge
 - Apartment / condominium
- Multi-Family Residential
 - Apartments and condominiums

Height / Mass

- Mixed-use: 3-story, 1-commercial, 2- residential
 - 300' maximum façade length
 - 100' façade breaks
 - 1st. floor vertical set-back
- Residential: 3-story
 - 300' maximum façade length
 - 100' façade breaks

Architecture

- Contemporary / modern
 - North Florida Brick Georgian, Brick Victorian Storefront, New Orleans, and Key West

Set-backs

- Mixed –use
 - Front - 10'
 - Sides – 80' street side, 0' other side
 - Rear – 80'
- Residential
 - Front - 30'
 - Sides – 0', 12' on the corners
 - Rear – 80'

Density

- Mixed – Use: FAR = 2:1
- Residential: 2:1

Lot Size

- Mixed – Use
 - Front line - 100'
 - Side line – 200'
 - 20,000 square feet minimum
- Residential
 - Front line - 200'

- Side line – 200'
- 40,000 square feet minimum

Parking

- Mixed-use:
 - Commercial; 1 parking space for 300 square feet
 - Residential: 2 space per unit
 - 1 bay required corner side yard parking
 - Remainder behind building
- Residential:
 - 2 spaces per unit
 - Underneath or behind building

Landscape

- Parking lot minimums
- Store-frontage minimums
- Street-front minimums

Pedestrian

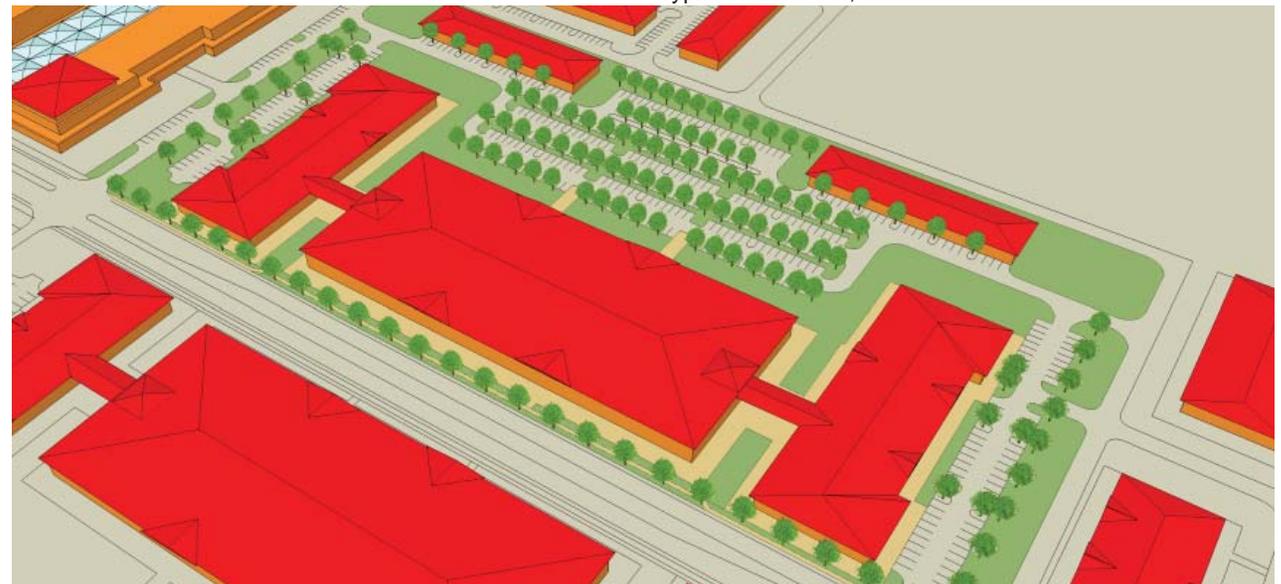
- Entry plazas
- Sidewalk minimums
- ADA accessibility



Typical multi-family residential



Typical mixed-use; 1-commercial and 1-residential



District 3d diagram



4

THE IMPLEMENTATION PLAN

Overview

The success of Callaway Central Corridors Urban Design Plan will require the coordinate efforts of the citizens, property and business owners, and City agencies. This implementation chapter defines the important steps that must be taken in order to realize the successful redevelopment of these commercial corridors. The redevelopment implementation process for this kind of project will take many years. Market conditions change, and the plan and the phasing will probably be updated many times before the project is ultimately completed. The zoning changes and LDR changes are important elements to get started immediately. The capital improvements can follow as funding becomes available.



Project Phasing Plan

The phasing plan portrays a projected schedule for planning, designing, constructing and managing all of the major elements related to the long-range implementation of the urban design plan. The phasing plan represents a logical chronological sequence of events to achieve the desired goals. The market conditions, developer participation, availability of grants and funding are but a few variables that can affect the speed with which these steps can be accomplished. The phasing plan is organized around 2 major areas of activity: Public sector work and Private sector work.

Public Sector Initiatives

Highway 22 Re-construction and Streetscape

- The Highway 22 reconstruction project will be a very large component of the corridor urban plan. Based upon the FDOT current projections for capital investment in the upgrading of this roadway, the soonest that the FDOT would have money available would 10 years from now. The City needs to be prepared for when these monies can become available. The best way to be prepared is to have the roadway design recommendations in this plan and to have a set of conceptual design plans prepared as soon as possible. With these tools in place, the City's TPO representative can lobby for the project and for making the funds available sooner than later. This plan shows the construction drawing phase of the project beginning in 2019.
- The gateways for the Highway 22 corridors can be what we call an early victories project. The CRA can design and construct these great image setting elements with a small amount of money and receive a large amount of impact. These gateways can be installed and not disrupt any future improvements. These early action elements are shown to begin in 2009 and be completed in 2011.

Tyndall Parkway Renovation and Streetscape

- The roadway improvements for Tyndall Parkway will not be as expensive as those required for Highway 22. As such, it will be easier to get the improvements on the FDOT plan. In order to lobby for the improvements with the FDOT, it will be important for the City to prepare the conceptual design drawings to help sell the ideas.
- The corridor gateway improvements for the Tyndall corridor can be early action items also.

The design and construction for the gateways is proposed for 2010.

- The storm water park (Hickory Street Park) project can begin after the initial master plan for the town center system is completed. Design and construction are projected to occur in 2012.

Town Center Park Construction

- The design and construction of the storm water park system is one of the highest priority projects for the corridor redevelopment. This is an important project because these critical improvements need to be in place before the redevelopment and new construction of the surrounding properties can begin. For this reason, these projects are required to move forward as soon as possible. The storm water master plan should be prepared in 2009. Land acquisition for pond improvements and park land should be underway in 2010 and 2011. During these years, we anticipate having the lowest property prices in decades. Construction drawings for the drainage system should occur in 2011 and construction of the storm water system should commence in 2012.
- Once the storm water drainage system has been constructed, then the park (Town Center Commons) design and construction should begin. The phasing plan calls for 3 park construction stages. The first will begin in 2013, the second will start in 2015, and the last to begin 2017.

Private Sector Initiatives

Zoning Code and LDR Up-date

- A very important first phase action item will be the necessary revisions to the Zoning Code and the Land Development regulations. Once the economy turns around, prospective developers will begin to start buying up properties along the corridor in anticipation of the "Next Wave" of land development. The City / CRA needs to have the new zoning

parameters in place prior to this speculation and purchasing wave. The investors need to know the “new” property rights associated with these corridor properties as well as the new design parameters before they purchase. The revisions to the codes should take about a year and should be initiated in 2009.

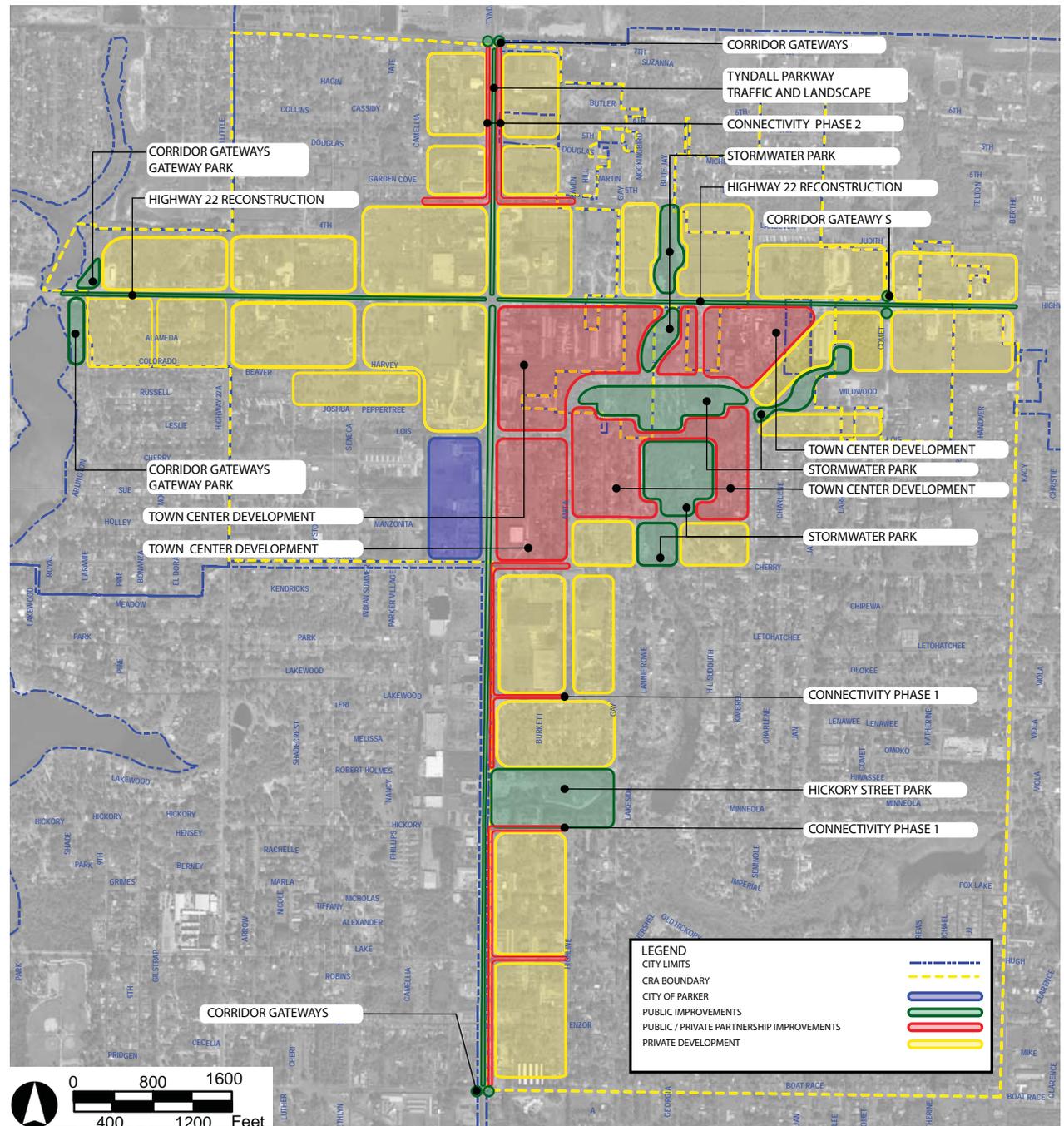
Tyndall Parkway Access-way Connectivity

- A critical element of the urban design strategy is the creation of an access-way to interconnect all business (present and future) in each block. Temporary alley ways will need to be constructed in order to provide this connectivity until each block is redeveloped. The time frame slated for this access-way connectivity project is from 2014 to 2017.

Town Center Development

- The Town Center in-fill project is the lynch-pin project for the plan. It will be acting as the catalytic project for the corridor. This will be a public-private partnership with the CRA and a developer. This is a very large undertaking and will require extensive planning and lead time to initiate. Because of the importance of this project as a catalyst, it is scheduled to begin immediately and continue on in phases for the next 20 years. The phasing plan allocates the next 3 years for property acquisition due to the current depressed market valuations. 2012 is the target date to formulate a developer team to begin the design and construction process. The 2012 date assumes that the market will begin to be ready for a project of this order by this time. There are 4 major town center development quadrants associated with the overall town center area. These will, in all likelihood, become separate development phases. Each of these will be a 3 year project and take 12 years to complete. Obviously, the exact phasing will be determined when a developer partner is identified.

Phasing Plan



Capital Improvements Plan

The Capital improvements plan consists of the individual projects that are a part of the above phasing plan and are expected to be financed in whole or in part with the TIF trust fund monies. The individual projects that are anticipated at this time will be as follows:

Highway 22 Construction

- Highway 22 construction
 - FDOT
- Gateway park
 - Design – \$80,000.00
 - Construction - \$800,000.00
- Corridor Gateways
 - Design - \$25,000.00
 - Construction - \$250,000.00
- Streetscape
 - Design – \$150,000.00
 - Construction - \$2,000,000.00

Tyndall Parkway Reconstruction

- Tyndall Parkway construction
 - B.I.D.
- Corridor Gateways
 - Design - \$25,000.00
 - Construction - \$250,000.00
- Streetscape
 - Design – \$200,000.00
 - Construction - \$3,000,000.00
- Hickory Street Park
 - Design – \$150,000.00
 - Construction - \$1,500,000.00

Town Center Park

- Stormwater System
 - Design – \$200,000.00
 - Construction - \$2,000,000.00
- Town Center Commons
 - Design – \$300,000.00
 - Construction – \$4,000,000.00

Zoning and Code Work

- Zoning Code and LDR re-write
 - Planning - \$ 100,000.00

Tyndall Parkway Access-way Connectivity

- Temporary Access-ways
 - Design – \$150,000.00
 - Construction – \$1,500,000.00

Town Center Development

- Developer Agreements - \$50,000.00
- Land Acquisition and Development – Developer Team

Funding Opportunities

Redevelopment efforts such as the Callaway central corridors plan one can be very expensive undertakings. The CRA of Callaway will need to be strategic in the ways that it utilizes and matches the important tax increment financing funds that it will generate over the years. One of the best ways to utilize the tax increment funds is to leverage them by matching the monies with grant dollars. In the best of economic times, the private sector development entities may be called upon to contribute fair-share public improvements. The following is a list of potential funding sources that can be incorporated into the search for matching funding.

Highway 22 Construction

- Highway 22 construction
 - FDOT, USDOT
- Gateway park
 - Florida Recreation Development and Acquisition Program (FRDAP)
- Corridor Gateways
 - Callaway CRA – Tax Increment Financing (TIF)
- Streetscape
 - FDOT – TEA-21

Tyndall Parkway Reconstruction

- Tyndall Parkway construction
 - B.I.D.
- Corridor Gateways
 - Callaway CRA – Tax Increment Financing (TIF)
- Streetscape
 - FDOT – TEA-21
- Hickory Street Park
 - Florida Recreation Development and Acquisition Program (FRDAP)

Town Center Park

- Storm Water System
 - Northwest Florida Water Management District
- Town Center Commons
 - Florida Recreation Development and Acquisition Program (FRDAP)
 - Florida Conservation Trust (FCT)
 - Florida Department of Environmental Protection (FDEP) – Greenways and Trails Program

Zoning and Code Work

- Zoning Code and LDR re-write
 - Planning – Callaway Tax Increment Financing (TIF)

Tyndall Parkway Access-way Connectivity

- Temporary Access-ways
 - Callaway Tax Increment Financing (TIF)
 - Business Improvement district (BID)
 - Gas Tax

Town Center Development

- Developer Agreements
 - Callaway Tax Increment Financing (TIF)
- Land Acquisition and Development
 - Developer Team



Implementation Schedule

Due to the extremely unpredictable nature of the current economic conditions and resulting tax revenue uncertainty. It is difficult to develop a precise timeline at this time. As the economy stabilizes over the next 2 years, a definitive schedule should be developed which can reflect more accurate revenue projections.

